

EXHIBIT 2

Chapter 17.5 - IH-35E Corridor Overlay District

Sec. 17.5-1. Definitions.

The following words, terms, and phrases, when used in this chapter, shall have the following meanings. For any word, phrase, or term not defined in this chapter, the definitions in the Zoning Ordinance and Land Development Regulations shall apply. If any definition in this chapter is in conflict with the Zoning Ordinance or Land Development Regulations, then the definitions in this chapter shall prevail:

Administrative Modification shall mean a requested modification of up to 10% of any numerical Standard established in this chapter, unless a more specific threshold has been established for Administrative Modifications in this chapter, but shall not include any requested modification to any Standards relating to development intensity (height and allowed square footage), density, or uses permitted.

Alternative Standards shall mean a requested modification of more than 10% of any numerical Standard established in this chapter, any modification of a non-numerical standard established in this chapter, or a Concept Plan which does not comply with Appendix D (Framework Plans) but shall not include any requested modification to any Standards relating to development intensity (height and allowed square footage), density, or uses permitted.

Amenity Zone shall mean that portion of the Public Right-of-Way in which the property owner is responsible for the placement of Sidewalks, trees, if required, and Street Furnishings in a manner that does not obstruct pedestrian access or motorist visibility.

Architectural Screens shall mean any permanent, semi-opaque screening device that is integrated with the architectural Facade design of a building. Architectural Screens may use a range of compatible materials including metal or plaster and may use opening patterns with geometric or irregular shapes.

Art shall mean sculpture, water features, and murals, or paving designs of a unique and attention-getting character.

Articulation shall mean features that provide architectural detail, differentiation, openings, and characteristics.

Awning shall mean a roof-like cover which may be fixed in place or retractable that is not a permanent integral element of the building to which is attached, projecting from the Facade of a building for the purpose of shielding a doorway or window from the elements.

Block Perimeter shall mean the sum total dimension of all sides of a block circumscribed by Streets/Private Vehicular Routes as measured along the property lines.

Build-to Zone shall mean an area of a specified depth, parallel to and touching a specified lot line or other setback line, within which the building's Facade shall be placed.

Building Height shall mean the overall height of a building as measured from natural grade

to the tallest projecting element on the building and/or Parking Structure.

Canopy shall mean a roof-like structure that is an integral element of a building and extends horizontally more than one (1) foot from the face of a building Facade.

Corridor District shall mean the IH-35E Corridor Overlay District, as created by this chapter.

Concept Plan shall mean a narrative, complete with illustrations, outlining a proposed project in detail. The Concept Plan shall show the applicant's intent for the use of the land within the proposed zoning district in a graphic manner, and as may be required, supported by written documentation of proposals. Submittal requirements shall be the same as those outlined for Concept Plans in the Zoning Ordinance.

Development shall mean the subdivision of land and/or construction, reconstruction, expansion, structural alteration, conversion, or relocation of any buildings or structures; and any extensions of use of land. Development includes New Development and Redevelopment. Minor improvements to an existing property or structure, including routine maintenance, aesthetic enhancements, and landscaping enhancements shall be excluded from this definition.

Driveways shall mean an opening along the curb line at which point vehicles may enter or leave the Street/Private Vehicular Route.

Engineering Site Plan shall mean the development plans required by the LDR for one or more lots upon which is shown all information required by this chapter and the LDR, sealed by a state-licensed civil engineer. There are specific requirements for Engineering Site Plans in the LDR. An architectural site plan shall not be substituted for an Engineering Site Plan.

Facade shall mean the portion of any exterior elevation on the building extending from grade to top of the Parapet, wall, or eaves and the entire width of the building's elevation but excluding the roof. Where separate faces are oriented in the same direction or in directions within forty-five degrees of one another, they are to be considered as part of a single Facade. Multiple buildings on the same lot will each be deemed to have separate Facades.

Facade Rhythm shall mean the repetition of certain elements (windows, doors, columns, etc.) in regular manner along a building's elevation.

Frontage Road (IH-35E Frontage Road) shall mean roads that run parallel to IH-35E that provide access to abutting landowners and distribute and collect traffic to and from the freeway interchange.

Graphic Representation shall mean any sketch, photograph, or similar representation.

Ground Floor shall mean that portion of a building from street-level finish floor elevation and extended twelve and one-half feet above the street-level finish floor elevation.

Hardscape shall mean paving materials having architectural design features or patterns and laying flush on the ground level.

Land Development Regulations or LDR shall mean the City of Lewisville Land Development

Regulations, Chapter 6 of the Code of Ordinances, as amended. Also referred to as the General Development Ordinance (GDO).

Local Street shall mean a Street providing the primary means of access to properties which shall generally accommodate low traffic speeds and volumes.

Masonry shall mean brick, stone, rock, stucco, plaster, cement, or concrete tilt wall installed in accordance with the city's adopted building code. It shall not include EIFS (Exterior Insulation and Finish System), Hardi plank, or materials of similar characteristics.

Mixed-use shall mean a tract of land, building, or structure which combines residential and nonresidential uses within a single site, building or structure. Mixed-use sites, buildings or structures may consist of two or more uses including but not limited to residential, office, retail, restaurant, civic or entertainment.

New Development shall mean the site preparation and construction of new facilities on property previously undeveloped.

Open Space shall mean an area of land set aside, dedicated, designated, or reserved for public or private use for recreational activities or other amenities, including parks, courtyards, plazas, patios, etc. Open Space may be maintained by either the property owner or the city.

Pad Sites shall mean outparcels for individual commercial (usually drive through restaurants) buildings generally surrounded by its own parking and/or Driveways within a larger project such as a shopping center.

Parapet shall mean that portion of a building wall or Facade that extends above the roof line of the buildings.

Parking Lot shall mean a paved surface with On-site Parking spaces at grade level.

Parking, Off-street or On-site shall mean parking located completely within private property.

Parking, On-street shall mean parking located completely or partially within a Public Right-of-Way, Private Vehicular Route or Street.

Parking Structure shall mean a parking garage located above ground and/or underground consisting of one or more levels but excluding a Parking Lot.

Pavers shall mean brick or other man-made masonry units including stamped concrete and asphalt pavers intended for surface paving specifically, though not exclusively, excluding split-faced block, or CMU (Concrete Masonry Units) block.

Primary Entrance shall mean an entrance from a Primary Pedestrian Street/Private Vehicular Route, if a building has frontage along a Primary Pedestrian Street/Private Vehicular Route, or an entrance customarily providing entry to a main lobby, waiting room or foyer, if the building has another Street frontage.

Primary Pedestrian Streets shall mean those Streets labeled as Primary Pedestrian Streets in Appendix D that are high quality pedestrian routes, that have a minimum Sidewalk width of 6 feet,

offer adequate buffers between moving vehicles and pedestrians, contain a higher level of pedestrian amenities, and link major destinations.

Principal Building shall mean a building, structure or other facility, or a combination thereof, which are designed for or occupied by a principal use.

Private Vehicular Route shall mean a way for traffic which functions as a Primary Pedestrian Street, and is situated within a privately owned and maintained cross or joint access easement.

Public Right-of-Way (ROW) shall mean any public street, highway, roadway, alley or Sidewalk dedicated to and maintained by any public entity.

Redevelopment shall mean the demolition of all or a portion of existing facilities and the construction of new facilities on the property.

Reflective Glass shall mean glass having a reflectance of greater than 10%.

Secondary Walkway shall mean a Walkway immediately adjacent to a building that is set back from the Street by a Parking Lot. Such a Walkway may be parallel to the Sidewalk.

Shared Parking shall mean parking that is utilized by buildings or tenants on two or more parcels.

Sidewalk shall mean a paved surface intended for pedestrians provided immediately adjacent to Streets/Private Vehicular Routes, which are open to the public.

Standards shall mean basic requirements for any Development in this chapter. These are in addition to the specific requirements of the LDR.

Street shall mean a way for traffic, whether designated as a street, highway, thoroughfare, parkway, road, boulevard, alley, or however other designated.

Streetscape or Streetscape Zone shall mean all elements that are located outside of the Travel Zone of the Street/Private Vehicular Route and may include the Amenity Zone and Sidewalks.

Street Furnishings shall mean any elements useful for pedestrian convenience and comfort including but not limited to: pedestrian lights, benches, newspaper racks, trash receptacles, bollards, planters, tree grates, fences, railings, bicycle racks, mailboxes, fountains, kiosks, and phones.

Travel Zone shall mean the portion of the Street/Private Vehicular Route dedicated to vehicular traffic, medians, bike lanes and on-street parking.

TxDOT shall mean the Texas Department of Transportation.

Utilities shall mean any utilities serving a building on private property or placed in Public ROW or public easements.

Walkway shall mean any pedestrian accommodation that provides internal connectivity within a site.

Zoning Ordinance shall mean Chapter 17 of this Code of Ordinances, as amended.

17.5-2. General Provisions

(a) Purpose:

This Corridor District is intended to implement the IH-35E Corridor Redevelopment Plan adopted in November 2014 by the Lewisville City Council. The purpose of the Corridor District is to guide New Development and Redevelopment along the IH-35E corridor by establishing enhanced Standards that increase the quality of development and encourage sustainable design while still maintaining the health, safety, and welfare of the public. The design regulations included in this Corridor District provide property owners and developers with a clear set of Standards that will instruct site planning, architecture, landscaping, Streetscapes, and other elements to create a consistent character of development throughout the Corridor District.

(b) Geographic Limits:

The geographic limits of the Corridor District are as shown in the Corridor District Map, attached hereto as Exhibit 1 in Section 17.5-3, and as more specifically described in the IH-35E Corridor Overlay District Boundary in Appendix H. Within the limits of the Corridor District, there are four core sub-districts: Northern Gateway, Main Street, Central, and Southern Gateway, as shown in Exhibits 2-5 in Section 17.5-3. All other areas within the Corridor District shall be part of the transition sub-district, as shown in Exhibit 1 in Section 17.5-3.

(c) Purpose and Intent Statements:

Purpose and intent statements are provided to set out high-level objectives.

(d) Standards:

Standards are provided to set out quantifiable measures, designed as regulations, to achieve the stated intent and purpose.

(e) Graphic Representation:

All Graphic Representations within this chapter are illustrative of the described Standard only and will not be used for regulation or enforcement of this chapter. The Standard noted in text shall prevail over any Graphic Representation.

(f) Applicability:

- (1) The regulations identified in this chapter shall apply to all properties within the Corridor District.
- (2) Table 1 illustrates the extent to which different sections of this chapter apply to any proposed New Development or Redevelopment.

- (3) The following Appendices shall be informational and illustrate the vision for Development in the Corridor District:
 - a. Appendix A: Corridor Character Principles;
 - b. Appendix B: Design Principles;
 - c. Appendix C: Core Sub-district Illustrative Plans

- (4) The following Appendices shall be regulatory and all applications for Development shall, as required by Table 1, meet the requirements in:
 - a. Appendix D: Core Sub-district Framework Plans.
 - b. Appendix F: Street Design Standards
 - c. Appendix G: Planting List

- (5) Relationship to the Zoning Ordinance and Land Development Regulations (LDR):
 - a. All provisions and requirements of the Zoning Ordinance and LDR apply to Development within the Corridor District, except where those provisions and requirements conflict with this chapter, in which case this chapter shall control.
 - b. Standards, requirements, and processes that are not addressed in this chapter shall be governed by the Zoning Ordinance and LDR.
 - c. All planned development district standards approved prior to the effective date of this ordinance shall supersede the Standards in this chapter.
 - d. Nothing in this chapter shall change the underlying zoning designation of any property within the Corridor District.

- (6) All Development including the application of the Standards contained in this chapter must comply with all applicable laws and regulations including, but not limited to, the Americans with Disabilities Act and Texas Accessibility Standards.

- (g) Process:** Development within the Corridor District shall follow the processes required by the LDR and all other applicable ordinances to the extent they are not in conflict with the Standards in this chapter and subject to the following (see Appendix E, Process Flow Chart, for Graphic Representation):
 - (1) Applications
 - a. Engineering Site Plans
 - 1. An Engineering Site Plan must be submitted to staff for review for any Development which is required to comply with the Standards in this chapter, when required by Table 1.
 - 2. Staff may approve Engineering Site Plans where Developments submitted conform to the requirements of this chapter and all applicable provisions of

the LDR.

b. Concept Plans

1. A Concept Plan shall be submitted as required by the Zoning Ordinance.
2. For any site that includes an area five (5) acres or greater or is part of a larger project such as an outparcel in a shopping center, a Concept Plan shall be submitted and approved by staff prior to any Engineering Site Plan approvals.
 - i. This requirement shall not apply if a Concept Plan has been previously approved as required by a zoning district designation.
 - ii. All Concept Plans shall be in compliance with the Framework Plans, outlined in Appendix D.
 - iii. A request for approval of a Concept Plan which does not comply with the Framework Plans in Appendix D shall be considered an application for an Alternative Standard and shall be handled and reviewed as such.

(2) Administrative Modifications

- a. A request for an Administrative Modification shall be submitted to staff at the time of submittal of the Engineering Site Plan. Said request may be made on the face of the Engineering Site Plan.
- b. Administrative Modification requests which involve development intensity (height and square footage permitted), density standards, or uses permitted in the Corridor District are considered zoning change requests and will be processed as such under the Zoning Ordinance.
- c. Staff review
 1. Staff may approve an Administrative Modification that is being requested for one of the following reasons, unless otherwise provided in this chapter:
 - i. To accommodate a site specific condition, including but not limited to, topography, vegetation, easements, utilities, existing improvements in good repair;
 - ii. To provide pedestrian, bicycle, or transit improvements; or
 - iii. To accommodate a phased Development of a site.
 2. In no case shall any Administrative Modification allow:
 - i. An increase in overall allowed intensity, density, height or lot coverage;
or
 - ii. A change in permitted uses or mix of uses.

(3) Alternative Standards

- a. A complete application for any Alternative Standard shall be submitted, in writing, to staff at least four (4) weeks prior to any Overlay District Board meeting.
- b. The request shall state fully the grounds for the application and all facts relied upon by the applicant. All supporting exhibits, fees and documents must be

included with the application. Incomplete applications will not be processed until all necessary documents are received by staff.

- c. Alternative Standards requests which involve development intensity (height and square footage permitted), density standards, or uses permitted in the Corridor District are considered zoning change requests and will be processed as such under the Zoning Ordinance.
- d. Overlay District Board review
 - 1. Standard of review
 - i. When reviewing applications for Alternative Standards, if the Overlay District Board finds that hardship or practical difficulties may result from strict compliance with the regulations in this chapter and/or the purpose of the regulations in this chapter may be served to a greater extent by Alternative Standards, it may approve or recommend exceptions to the Corridor District regulations so that substantial justice may be done and the public interest secured, provided that such exception shall not have the effect of nullifying the intent and purpose of the regulations in this chapter.
 - ii. The Board may refer to the *IH-35E Corridor Redevelopment Plan* and the appendices hereto in determining the intent and purpose of the regulations in this chapter. Issues the Overlay District Board may consider in making their findings include, but are not limited to the following:
 - aa. Whether strict compliance would impair the architectural design or creativity of the project;
 - bb. Whether strict compliance would create an unnecessary hardship or a practical difficulty which is unique to the Development and is not self-imposed;
 - cc. Whether an Alternative Standard is necessary to ensure compatibility with surrounding developed properties while still meeting the vision for the Corridor District; or
 - dd. Whether the proposed Development is an addition to an existing facility that does not meet the requirements of the Corridor District but meets the intent of this chapter.
 - 2. Authority
 - i. Final authority
 - aa. The Overlay District Board shall have the authority to make a final decision to approve or deny applications for Alternative Standards which are:
 - I. Modifications of more than 10% but less than 25% of a numerical Standard pertaining to landscaping or building materials and modifications of more than 10% but less than 60% of any other numerical Standard established in this chapter; or

c. When reviewing variance requests, the Board and the city council shall use the standards outlined for reviewing variances in section 6-31(a) of the LDR.

(5) Fees

An administrative fee for processing Alternative Standards requests and variance requests are applicable for all requests in an amount as set forth in the city's fee ordinance (section 2-201).

**Table 1.
 Applicability
 Matrix**

Regulations →	Building and Envelope Regulations							Architectural Regulations						Landscape Regulations				Street and Streetscape Regulations					
	Placement	Orientation	Blocks/Lots	Height	Driveways	Ped-Circulation	Parking	Articulation	Facades	Materials/Colors	Awnings/Canopies	Windows	Parking Structures	Tree/Plant Materials	Hardscaping	Open Space	General	New or Existing Streets	Sidewalks	Trails	Street/Ped Lighting	Street Furnishings	Screening of Service Areas and Utilities
(a) New Development*	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■
(b) Change of use/expansion of existing use (with NO increase in building area) (new use shall comply with the underlying zoning district)**							■																
(c) Interior remodel with no change of use, no change in any Street facing Facade, no increase of any existing nonconformity and no increase of building area																							
(d) Facade changes to existing buildings (regardless of value of improvements proposed)																							
(1) Addition of non-air conditioned space such as patios, porches, arcades, canopies, and outdoor seating areas (shall be permitted so long as no existing nonconformity is increased)**	■	■				■	■				■												
(2) Changes to any Street facing Facades (Standards in the specific section shall apply)**								■	■	■	■	■		■									■
(e) Expansion of Building Area																							
(1) 0% - 49% increase in building area regardless of value of improvements (Standards in applicable sections shall <u>apply only to the expansions</u>)*	■	■	■	■	■	■	■	■	■	■	■	■		■	■		■	■	■	■	■	■	■
(2) 50% or greater increase in building area BUT <i>less than both</i> : a. 50% increase in value of improvements, and b. Total proposed improvements valued at \$100,000 (Standards in applicable sections shall <u>apply only to the expansions</u>)*	■	■	■	■	■	■	■	■	■	■	■	■		■	■		■	■	■	■	■	■	■

Regulations → Development Application/Request	Building and Envelope Regulations							Architectural Regulations						Landscape Regulations			Street and Streetscape Regulations							
	Placement	Orientation	Blocks/Lots	Height	Driveways	Ped-Circulation	Parking	Articulation	Facades	Materials/Colors	Awnings/Canopies	Windows	Parking	Tree/Plant Materials	Hardscaping	Open Space	General	New or Existing Streets	Sidewalks	Trails	Street/Ped Lighting	Street Furnishings	Screening of Service Areas and Utilities	
(3) 50% or greater increase of building area AND <i>more than either</i> : a. 50% increase in value of improvements <u>or</u> b. Any proposed improvements valued at \$100,000 (Standards in applicable sections shall apply to the <u>entire site, including retrofitting of the existing building and site</u>).*	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■	■
(f) Expansion of Parking Lot only (not in conjunction with a building or use expansion)																								
(1) Up to 10 additional spaces (shall not be placed in any area that increases any existing nonconformity)**					■									■										
(2) 11 or more additional spaces (shall not be placed in a manner that increases any existing nonconformity)*					■	■	■							■	■									

* Engineering Site Plan shall be required

** Engineering Site Plan may be required based on the extent of proposed improvements

17.5-3. Corridor District Map

Exhibit 1. Corridor District Map

Exhibit 2. Northern Gateway Sub-District Map

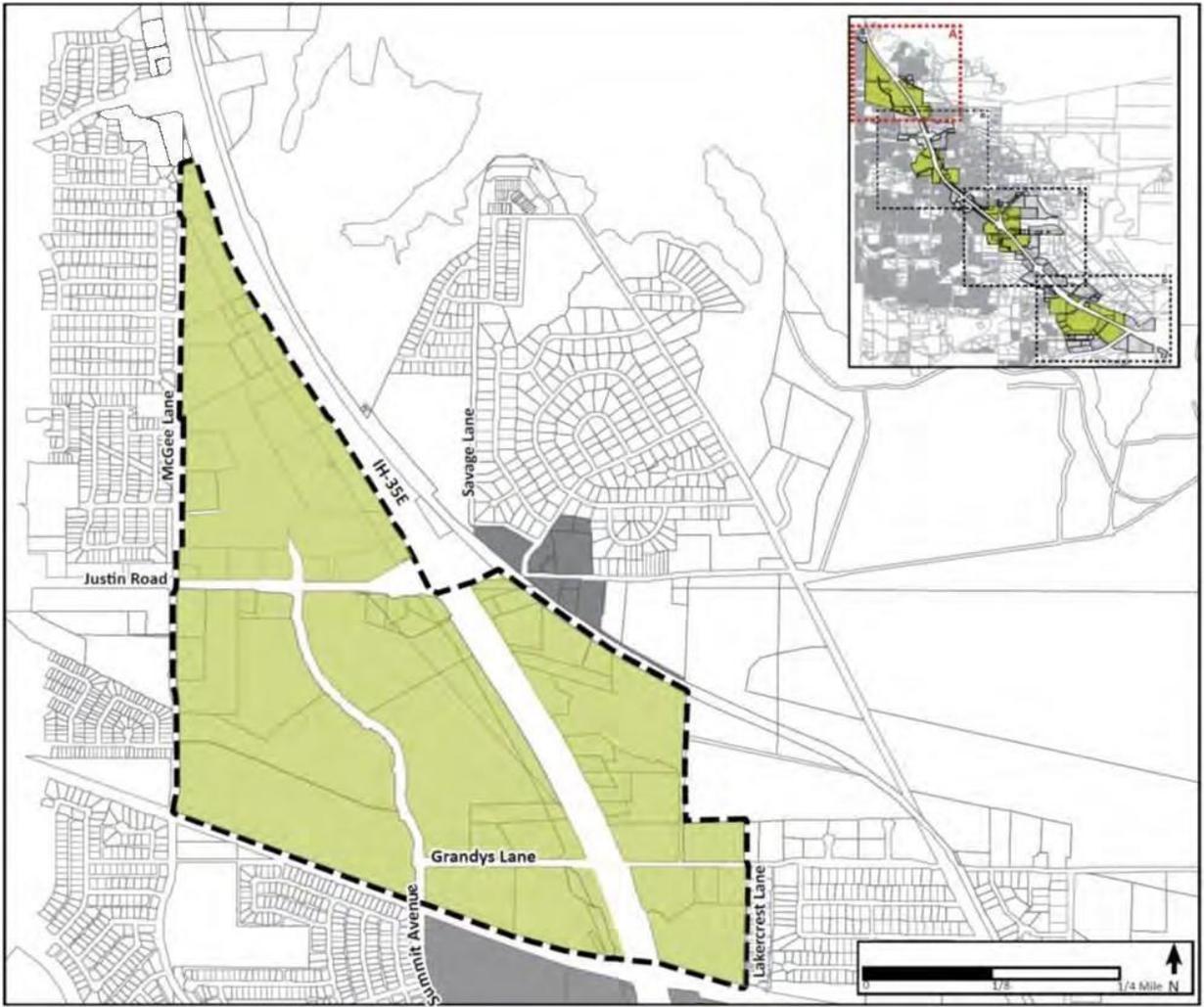


Exhibit 3. Main Street Sub-District Map

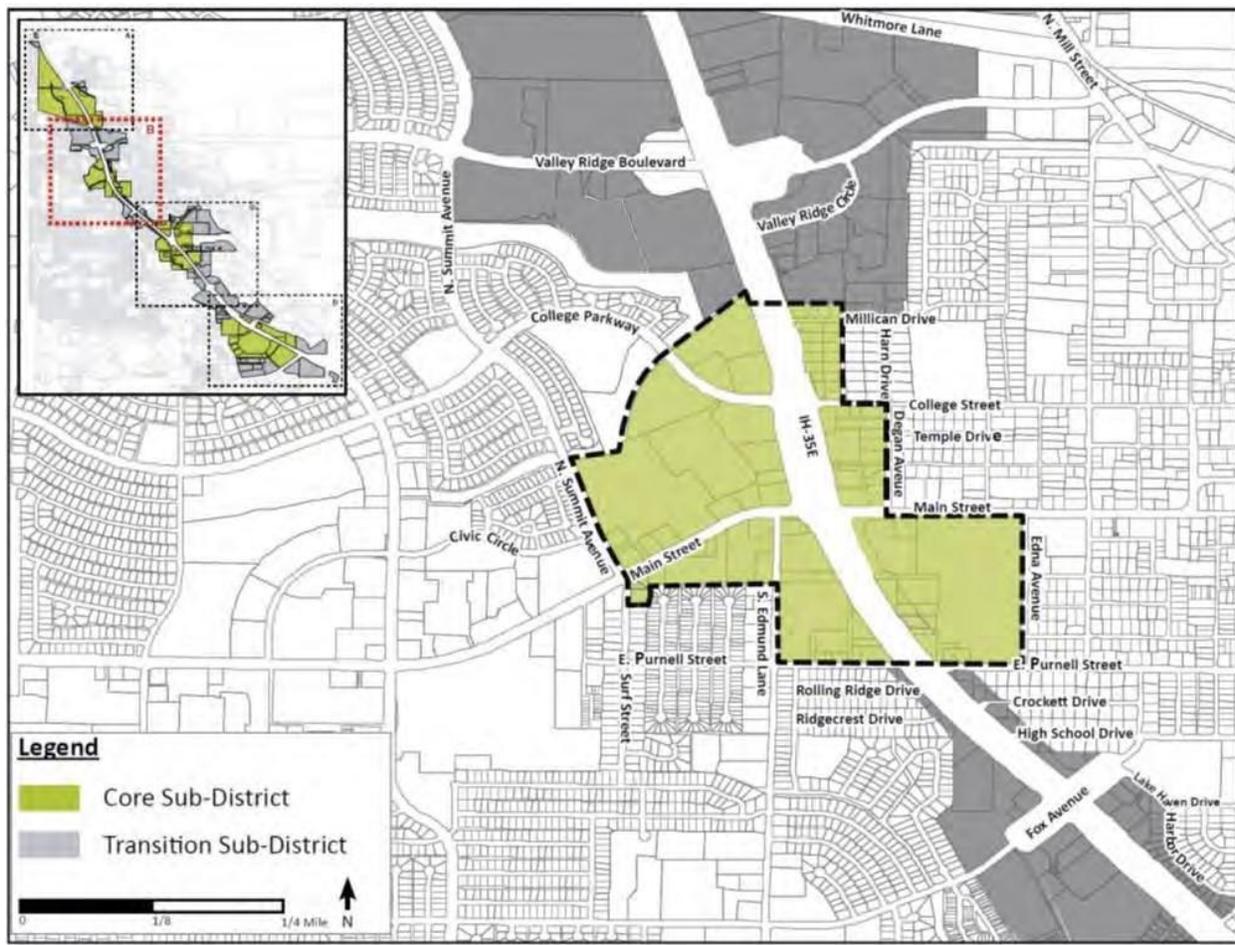


Exhibit 4. Central Sub-District Map



Exhibit 5. Southern Gateway Sub-District Map



17.5-4. Core Sub-District Regulations

(a) Purpose, Intent and Applicability:

- (1) Purpose and intent: The vision for the core sub-districts is to create a series of vibrant, districts with a mix of uses at different strategic locations along the Corridor District by:
 - a. Focusing on creating a pedestrian-oriented, more walkable environment along identified Primary Pedestrian Street/Private Vehicular Route frontages (as illustrated in the Framework Plans, Appendix D);
 - b. Creating memorable destinations that both capitalize on existing strengths while tying nodes of activity together;
 - c. Creating regional gateways which are enhanced with a range of uses including living, working, and green spaces; and
 - d. Encouraging higher densities with a vibrant mix of uses that leverage expanded transportation options, allow higher land utilization, and provide a higher tax base.

- (2) Applicability: The regulations in this section, as illustrated in Table 1, apply to any property within the core sub-districts as shown on Exhibits 1-5 in Section 17.5-3.

(b) Building and Envelope Standards:

These Standards establish where the building should be placed on the property with respect to setbacks and build-to lines, the orientation of a building's Facades, alignment of new Streets/Private Vehicular Routes and blocks, height of buildings, Driveways and parking, and pedestrian accommodations on the site.

- (1) Building placement:
 - a. Building fronts and sides shall be placed parallel to adjacent Streets/Private Vehicular Routes to the extent practical with the highest priority placed on the front Facade of the building.
 - b. Build-to Zones and building setbacks shall be measured from the property line and shall be provided in accordance with the requirements in Table 2 of this subsection.
 - c. The building frontage build-out shall be in accordance with the requirements in Table 2 of this section. The building frontage build-out is the length of the portion of the Principal Building and/or Parking Structure which shall be located within the required Build-to Zone, as a percentage of the total width of the lot line.
 - d. At intersections, buildings shall have a minimum of at least twenty-five (25) feet of building frontage build-out along both Streets.
 - e. Canopies, signs, Awnings, and balconies may encroach over Walkways or over a

Build-To Zone area as long as the vertical clearance is a minimum of nine (9) feet. In no case shall an Awning or Canopy encroach over a Driveway or fire lane.

- f. **Specific Standard for Administrative Modification:** For properties fronting the IH-35E Frontage Road and arterial roadways, an Administrative Modification may be requested to allow an eighty-foot maximum Build-to Zone to allow for a single drive aisle with parking between Street and Principal Building as long as the building placement otherwise meets the standard in subsection (b) above.



Ground floor storefront is built to the property line, defining the Street edge.

Table 2. Building Placement and Frontage Build-out Standards				
	Build-to Zones		Building Frontage Build-out (min.)	Off-street Parking permitted between building and Street
	Minimum (feet)	Maximum (feet)		
Front (by Street Type)				
Primary Pedestrian Street/Private Vehicular Route	0	10	80%	No
IH-35E Frontage Road	10	80*	0%#	Yes*
Arterial roadway	10	80*	0%#	Yes*
Collector or Local Street (2-4 Lanes)	10	80	None Req'd	Yes
Other or Alley	10	None	None Req'd	Yes
Side (interior)	0**	None		
Rear (non-alley)	0**	None		
*Through Administrative Modification				
**There shall be no minimum setback unless the building is adjacent to single family residential uses that are outside the Corridor District, in which case the minimum setback shall be 20 feet.				
# If the site only has only one Street frontage (IH-35E Frontage Road or arterial roadway), then the longer side of the building shall be placed parallel to the Street.				

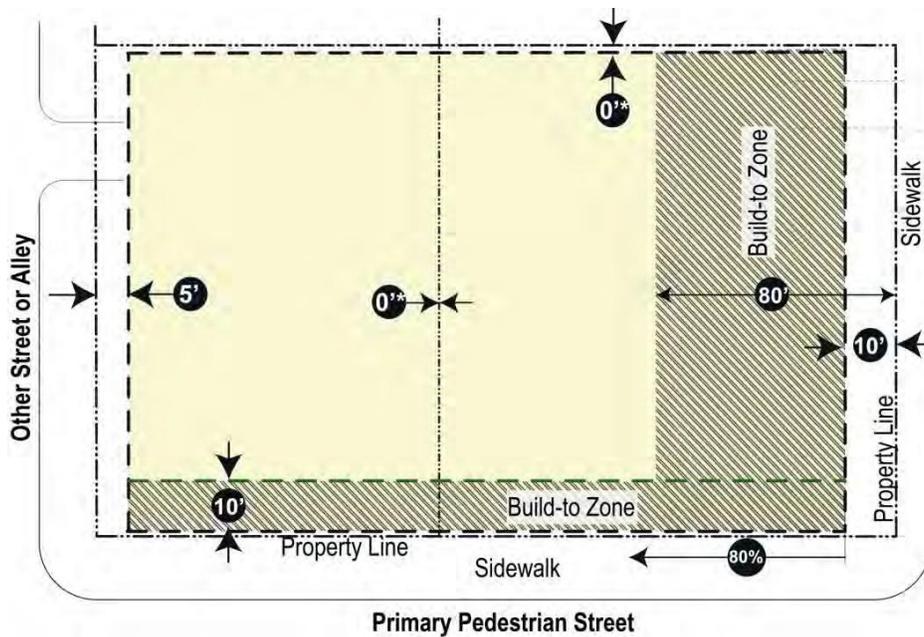


Image showing Build-To Zones

- (2) Building orientation:
 - a. Primary Entrances:
 1. All Primary Entrances to a building shall be oriented towards a Primary Pedestrian Street/Private Vehicular Routes.
 2. If the building does not have frontage on a Primary Pedestrian Street/Private Vehicular Route, the Primary Entrances shall be oriented toward Open Space, parks, and other significant spaces, where available. A building may have a secondary entrance from a Parking Lot or other Street.
 3. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested for buildings that do not have frontage on a Primary Pedestrian Street/Private Vehicular Route, Open Space, park, or other significant space.
 - b. Where a building is located along a Primary Pedestrian Street/Private Vehicular Route, the front of the building shall be oriented towards the Primary Pedestrian Street/Private Vehicular Route.
 - c. Off-street Parking shall not be located between any building and any Street/Private Vehicular Route unless permitted per Table 2 above.



Front Facades of buildings shall be oriented towards a Primary Pedestrian Street, parks, Open Space, and other significant spaces, where available.

- (3) Blocks and lots:
 - a. All blocks shall provide service and parking access along an alley or service drive.
 - b. The maximum Block Perimeter shall be 1,600 feet unless the Engineering Site Plan or Concept Plan follows the Street network as established by the Framework Plans (Appendix D).
 - c. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow for a 20% increase to the maximum Block Perimeter requirement.
 - d. Block faces that are 600 feet in length or greater shall provide pedestrian access through the block at an approximate mid-block distance, in order to allow pedestrians to walk through the blocks to the opposite side without having to walk along the Block Perimeter.

- (4) Building Height:
 - a. The minimum Building Height of a building or Parking Structure shall be 20 feet.
 - b. Minimum interior Ground Floor height shall be 12 feet (clear).

- (5) Driveways:
 - a. All vehicular entrances shall be located off of a Local Street or an alley. Driveways for service vehicles shall be via alleys or Parking Lots. If a property has no access to a Local Street or an alley, Driveways may be permitted along other Streets with the exception of Primary Pedestrian Streets/Private Vehicular Route.
 - b. Cross or joint access easements:
 - 1. Cross or joint access easements shall be required for all Developments unless grade and topography make such cross access unfeasible.
 - 2. Cross or joint access easements shall be designated at the time of Development in anticipation of future direct or indirect access.

- c. Driveway access may be permitted along Primary Pedestrian Streets/Private Vehicular Routes only if the property has no direct or indirect access (via a cross or joint access easement through an adjoining property) to any other Street. Such a Driveway access along a Primary Pedestrian Street shall be a maximum of 24 feet wide with a 20-foot radius and be deemed temporary and shall be closed when alternative direct or indirect access is provided to the property.
- d. For lots fronting the IH-35E Frontage Road and state highways, Driveways shall adhere to the Access Management Policy in the LDR. For all other lots, Driveways shall comply with the LDR, except that all Driveways shall be a maximum of 30 feet wide with a 20-foot radius.
- e. Sharing and consolidation of Driveways is encouraged in order to reduce the number of curb cuts to ensure smoother, more organized traffic movements and less disruption of pedestrian movement.
- f. Except where otherwise required by the Access Management Policy in the LDR, curb cuts (Driveway openings) shall:
 - 1. Be a minimum of 300 feet apart from any other curb cut or Street/Private Vehicular Route intersection; and
 - 2. Not interrupt the paving material of the Sidewalk with another material. Sidewalk paving shall be continuous across the Driveway.
- g. Driveway entrances into multi-building commercial properties along the IH-35E Frontage Road and all arterial roadways shall be enhanced with the addition of signs, accent paving, special landscaping and/or lighting. Design elements shall not be located within any required visibility easements or clear vision areas.

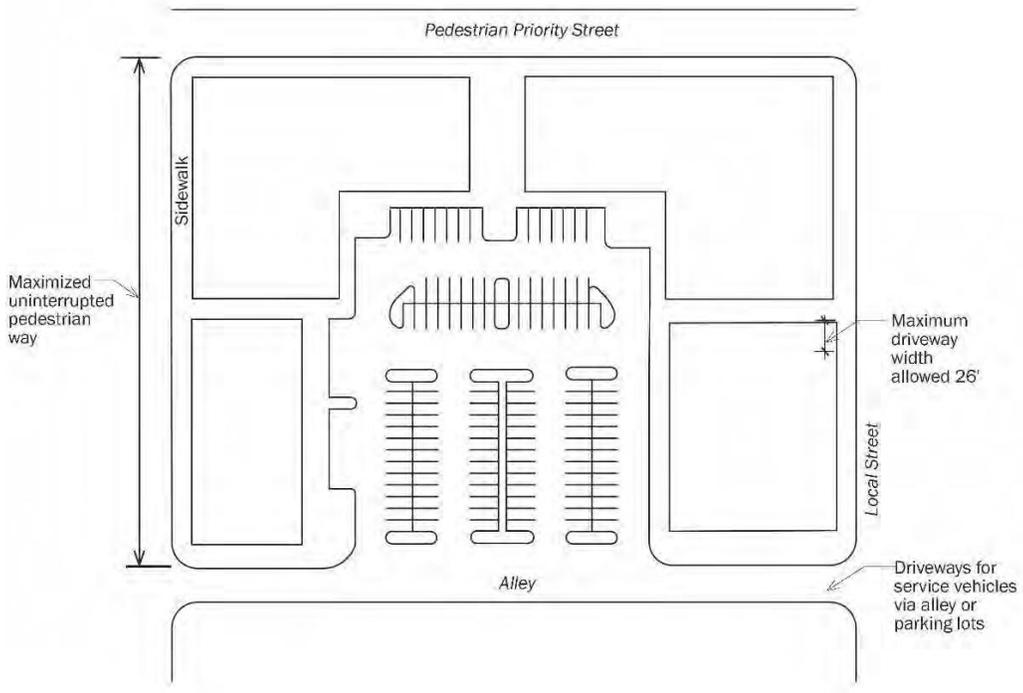


Image showing acceptable Driveway locations

- (6) Pedestrian circulation
 - a. Sidewalks
 - 1. Sidewalks for separate and continuous pedestrian circulation shall be provided along all Streets/Private Vehicular Routes.
 - b. Walkways
 - 1. Walkways shall be no less than six (6) feet in width.
 - 2. Walkway connections shall be required:
 - i. Between any existing or future Sidewalk, trails, parks or greenways and Primary Entrances of all buildings on the site;
 - ii. From building entrances to all on-site facilities, such as Parking Lots, bicycle facilities, and Open Space;
 - iii. Up to the property line in a manner which will allow walkway connections between adjoining properties and buildings; and
 - iv. To connect to any public transit stop that is adjacent to a site.
 - 3. Walkways shall be distinguished from any driving surfaces through the use of colored pavements, bollards, grade changes, pavement markings or combination of treatments, especially when Walkways cross or are adjacent to vehicular circulation areas.
 - c. Parking
 - 1. Parking Lots and Parking Structures shall clearly mark Walkways between parked vehicles and Primary Entrances through the use of clearly marked stop

signs, wayfinding/directional signage, lighting, and other similar measures.

2. Parking stalls shall not overhang onto any Walkway, Sidewalk or landscape area and shall require wheel stops or an increase in the minimum Walkway, Sidewalk or landscape area of at least the depth of the overhang.

(7) Off-street parking

- a. Parking Lots shall be located behind or along the side of buildings. In no case shall parking be permitted within the required Build-to Zone with the exception of properties with frontage along the IH-35E Frontage Road, as specifically permitted by this section. Parking Lots may be located adjacent to existing single-family residential uses located outside of the Corridor District if they meet the following standards:
 1. Parking is set back a minimum of ten (10) feet from the single-family residential property line, and
 2. A masonry, vegetation or combined screening device that is at least six (6) feet in height shall be located at the property line along the single family residential use.
- b. On-site Parking shall be provided in accordance with the following parking requirements:
 1. Non-residential uses: one (1) space per 300 square feet of gross floor area.
 2. Residential uses: one and a half (1.5) spaces per residential unit.
 3. Mixed-uses: use ratios above to calculate required spaces based on composition of the uses on the property.
- c. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow credit for available public parking or shared Off-street Parking on another lot within 1,000 feet of the subject property to apply towards On-site Parking requirements.
- d. Centralized parking locations throughout the core sub-districts that permit people to park at convenient locations with ample parking to access multiple uses are encouraged, but not required.
- e. Shared Parking Lots shall be connected to businesses with paved and landscaped Walkways per subsection 17.5-4(b)(6), above.
- f. Shared parking agreements shall be submitted for review with the Engineering Site Plan and recorded with Denton County.

- (8) On-street Parking is required within the core sub-districts along all Primary Pedestrian Streets/Private Vehicular Routes, and encouraged, but not required, on collector and Local Streets.

(c) **Architectural Standards:**

These Standards apply to the exterior architectural treatments within the core sub-districts.

External architectural treatments include building Articulations, Facade composition, exterior materials and colors, windows and doors, and Awnings and Canopies. These Standards apply to commercial, Mixed-use, and multi-family buildings in addition to special Standards for Parking Structures. Buildings are the largest impact on the visual appeal of the core sub-districts and have the opportunity to set a high standard for the entire Corridor District.

(1) Building Articulation and Facades:

- a. Building Facades visible from any Street/Private Vehicular Route (except alleys) or Open Space shall provide horizontal and vertical Articulation with a Facade Rhythm between twenty and thirty feet. The Facade Rhythm may be expressed by any of the following:
 1. a change in a building's horizontal and/or vertical plane,
 2. stepping portions of Facades in and out;
 3. utilizing balconies, columns or pilasters that are distinctively set out from the Facade; or
 4. changing types or colors of materials in combination with other techniques.
- b. Each building Facade visible from any Street/Private Vehicular Route or public Open Space shall provide architectural variety and scale through the use of elements including, but not limited to at least three of the following:
 1. expression lines denoting the base, middle, and top of a building;
 2. repetition in patterns of window, door or other openings or architectural elements;
 3. change in color;
 4. change in texture;
 5. change in material module or pattern; or
 6. art or ornament constructed as part of the building.
- c. A minimum of 25% of above-grade residential units adjacent to a Street/Private Vehicular Route or public Open Space shall have balconies that extend a minimum of five (5) feet beyond the face of the Facade. Balconies may extend over the Sidewalk area provided they maintain a minimum of ten (10) feet of clearance above the Sidewalk and do not substantially interfere with tree growth.
- d. All buildings with more than one Street frontage shall treat all Street-fronting Facades with equal design attention.
- e. All building Facades along Primary Pedestrian Streets/Private Vehicular Routes and Facades immediately adjacent to a Sidewalk at-grade shall have:
 1. A minimum of 40% of the Ground Floor Facade comprised of windows and doors, except that buildings with retail or restaurant uses on the Ground floor shall have a minimum of 60% and a maximum of 80% of the Ground Floor Facade comprised of windows; and

2. Entries covered with Awnings, Canopies, or inset behind the front Facade a minimum of six (6) feet. A door shall not be permitted to swing into a Public Right-of-Way or minimum Sidewalk area.
3. Upper floors of all Facades facing Primary Pedestrian Streets/Private Vehicular Routes shall have windows for a minimum of 25 percent and maximum of 60 percent of the upper floor Facade area.

(2) Building entrances:

- a. Primary Entrances for single-tenant building entries shall be emphasized with dramatic architectural elements such as horizontal and vertical Articulations with Canopies, tower elements, recessed entries, pilasters, changes in height or materials, enhanced lighting, etc.
- b. Primary Entrances for multi-tenant buildings shall be emphasized through such design devices as Awnings, differentiation in material and/or color, and/or building signage.
- c. A minimum of one entrance shall be required for every 60 feet of multi-tenant storefront along Primary Pedestrian Streets/Private Vehicular Routes.
- d. For Mixed-use buildings with residential units, one or more separate building entrances from the Sidewalk shall be used to provide access to the residential units.



Ground floor commercial businesses are differentiated using vertical breaks and changes in building color and materials while maintaining an overall design theme.

(3) Facade materials and colors

- a. Facade materials: Each exterior wall (except window and door area) of a building or Parking Structure shall comply with the following material requirements:
 1. Eighty percent (80%) or more of each exterior wall shall be comprised of brick or stone. The remainder of each exterior wall may be comprised of either three-step stucco (EIFS shall only be allowed at locations nine (9) feet above grade), architectural metal, cladding, or panels, or a combination

thereof.

2. Accent materials may be comprised of pre-cast stone, metal, or other architectural materials.
- b. Colors: At least two (2) main colors, and no more than three (3) colors, shall be used on each elevation. Eighty percent (80%) of each elevation must utilize natural and/or earth tone colors, and each elevation must have 20% of another type of color. Buildings shall not have colors that are fluorescent or bright (e.g. yellow, green, purple) visible from any public space or Street/Private Vehicular Route. This shall include service doors, down spouts, utility boxes, panels and other similar features integral to the Principal Building.



Buildings materials as well as architectural details and finishes convey a sense of permanence. Quality materials shall be used to withstand the test of time regardless of architectural style.

- (4) Awnings and Canopies
 - a. All non-residential uses adjacent to the Sidewalk at-grade along Primary Pedestrian Streets/Private Vehicular Routes shall have an Awning or Canopy which extends beyond the face of the building over the adjacent Sidewalk for a minimum of 75% along the building's Sidewalk frontage.
 - b. Awnings or Canopies on any building shall be constructed of metal or canvas. Reflective materials for finishes are prohibited.
 - c. An Awning or Canopy shall be located as to provide shading for exterior windows and doors and shall cover the entire width of the window opening or group of windows over which it is installed.
 - d. Awnings and Canopies may encroach over Sidewalks up to 50% of the width of the Sidewalk or five (5) feet, whichever is greater. They shall maintain a minimum nine-foot (9') vertical clearance as measured between the bottom of the Awning/Canopy and the finished grade of the Sidewalk. In no case shall an

Awning or Canopy encroach over the Travel Zone.



*Awnings should fit into the storefront's overall architecture and appearance.
Awnings provide protection and cover from bright sunlight and inclement weather.*

(5) Windows

- a. Window framing materials shall consist of anodized aluminum, vinyl, or steel.
- b. For required windows at the Ground Floor along Primary Pedestrian Streets/Private Vehicular Routes, a minimum 60% visible light transmittance shall be required.
 1. **Specific Standard and Findings for Administrative Modification:** For windows along Street frontages other than Primary Pedestrian Streets, a visible light transmittance less than 60% may be allowed with an Administrative Modification if a finding is made by staff that an Administrative Modification is necessary in order for the windows to satisfy building code requirements related to energy efficiency.
- c. Mirrored glass is prohibited.
- d. Reflective Glass is prohibited on Ground Floors along Primary Pedestrian Streets/Private Vehicular Routes. Excluding Ground Floors along Primary Pedestrian Streets/Private Vehicular Routes, no more than 50% of any Facade may be Reflective Glass.
- e. Window screens are prohibited on non-residential Ground Floors.
- f. Horizontal groupings of windows are not to exceed five (5) per grouping, where groupings are separated by a mullion, column, or wall section a minimum of seven-inches wide.
- g. Windows shall be a minimum of thirty inches from building corners, unless separated by a corner mullion or column twice the width used in grouping.
- h. For storefronts along Primary Pedestrian Streets/Private Vehicular Routes, the following are prohibited:
 1. Single pane glass windows;
 2. Black glass, opaque glass, and other 'false window' techniques;

3. Doors with no opacity; and
4. Windows which do not permit unobstructed views into the building.



Windows should maximize visibility to the Street on the Ground Floor. Ensure that commercial Ground Floor uses provide clear unobstructed windows, free of reflective coatings.

(6) Parking Structures

- a. Parking Structures shall have a Facade design with the same colors and materials as the building the Parking Structure Services. If the Parking Structure serves multiple buildings, it shall have a Facade design with the same colors and materials as surrounding buildings along the Street front. Parking Structures shall be located behind buildings to minimize their visibility from adjacent Streets/Private Vehicular Routes.
- b. Parking Structures shall not be located with Ground Floor frontage along any Primary Pedestrian Street/Private Vehicular Route.
- c. Where a Parking Structure is located adjacent to a Street which is not a Primary Pedestrian Street:
 1. Parking Structure Facades shall be designed with both vertical Articulation such as changes in planes, columns, pilasters, etc. at least every 40 linear feet, and horizontal Articulation aligning with horizontal elements along the block.
 2. The Parking Structure shall be designed and screened in such a way that motor vehicles on all parking levels are hidden from view from all adjacent Streets. Parking Structure ramps shall not be visible from any Street. Ramps shall not be located on the perimeter of the Parking Structure. Architectural Screens shall be used to articulate the Facade, hide parked vehicles, and shield the lighting inside the structure.



Parking Structures shall be located behind buildings to minimize their visibility from adjacent Streets.



Parking Structures shall have a Facade design similar to surrounding buildings along the Street front.

(d) Landscape Standards:

These Standards apply to new trees and shrubs, Hardscape and Open Space treatments, screening of service, parking and utilitarian uses, and Parking Lot landscaping within the core sub-districts. Landscaping can reduce the urban heat island effect, soften the built environment and contribute to pedestrian comfort in addition to increasing property values and rents.

(1) Trees and plant materials

- a. Plantings shall be balanced symmetrically across Streets/Private Vehicular Routes or Driveways.
- b. Trees and plant materials shall be selected by the landscape architect or designer designing the landscape plan from the Approved Plant Material List in Appendix G. No artificial trees, shrubs, ground covers, turf, or seasonal colors are permitted.
 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to use other species that are drought tolerant and adaptive.
- c. Irrigation systems shall be provided within the Amenity Zone and shall be installed and maintained by the developer or property owner per current Parks and Recreation Department standards.
- d. Canopy trees shall be:
 1. Planted within the required Amenity Zone;
 2. Planted along Primary Pedestrian Streets/Private Vehicular Routes at an average spacing of thirty feet on center, but in no case placed more than forty feet apart on center;
 3. Planted along all other Streets (excluding alleys) at an average spacing of

forty feet on center; and

4. A minimum two and a half (2^{1/2}) inch caliper and at least ten (10) feet in height with a single trunk at planting.
 - e. Tree preservation and landscape maintenance shall comply with the provisions in the LDR.
 - f. Parking Lots with frontage on IH-35E shall provide a minimum ten-foot wide landscape area between the Public ROW and Parking Lot. Canopy trees shall be placed at an average spacing of forty feet, but in no case shall trees be placed greater than fifty feet apart. A solid evergreen shrubbery hedge with a minimum 24-inch height at the time of planting shall also be planted in a manner which will screen the view of parked vehicles from the Street.
 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow any of the following (minimum height of 36-inches) as a Parking Lot screen:
 - i. Retaining wall;
 - ii. Berming; or
 - iii. A combination of retaining wall, berming, and/or evergreen screening hedge.
 - g. Parking Lots along all other Street/Private Vehicular Route frontages (except alleys) shall use one or both of the following screening methods:
 1. A minimum five-foot wide landscape area between the Sidewalk and Parking Lot, with a solid evergreen shrubbery hedge with a minimum height of 24 inches at the time of planting planted within the landscape area in a manner which will screen the view of parked vehicles from the roadway; or
 2. A Masonry (brick or stone) wall a minimum of 36-inches in height installed between the Sidewalk and the Parking Lot, placed such that a minimum two-foot parking space overhang is provided on the Parking Lot side of the wall, and wheel stops are provided for the parking spaces.
 - h. All Parking Lots shall include the following landscaping elements:
 1. Each row of parking shall be configured so that there is a minimum ten-foot wide landscape island with a minimum of 50% plant cover every eight (8) parking spaces. A landscape island shall be required on the end of each row of parking.
 2. Each Parking Lot shall include canopy trees at a ratio of one (1) tree per eight (8) parking spaces.
- (2) Hardscaping
- a. All hardscaping materials shall be of earth tones or colors found on the Facade of the Principal Building.
 - b. Walkway paving shall be comprised of the following materials or a combination thereof:

1. Concrete;
2. Pavers; or
3. Stamped concrete or asphalt.

(3) Open Spaces

- a. Open Spaces shall be provided on Developments that incorporate five (5) acres or more.
 1. If the Open Space provided is not publicly accessible, the minimum set aside for Open Space shall be 15% of the Development. The Open Space shall be centrally located and easily accessible to all individuals it is expected to serve.
 2. If the Open Space provided is publicly accessible, the minimum set aside for the Open Space shall be 8% of the Development. The Open Space shall be highly visible from the Public ROW and accessible to the general public.
 3. If the proposed Development is impacted by any Open Space as identified in the Framework Plan, then the location of the Open Space shall comply with the Framework Plan.
 4. The Engineering Site Plan or Concept Plan shall demonstrate how the Open Space requirement is being satisfied for an entire Development being considered. For phased Developments, a plan may be approved if at least 50 percent of the required Open Space is constructed within the first phase of the project, with the remaining required Open Space being provided for in subsequent phases.
- b. Developments that incorporate less than five (5) acres may, but are not required to, incorporate Open Spaces for the use of employees or the public.
- c. Private Open Spaces may be fenced or otherwise controlled for secure access using wrought iron, Masonry, or comparable decorative fencing, or vegetative screening.
- d. Parking, Driveway, or rear setback areas may not be used as publicly accessible Open Space, but publicly accessible Open Space may include front and side setback areas provided that they are integrated into the overall design of the project. Publicly accessible Open Space shall have Street frontage for a minimum of 25% of the perimeter of the Open Space.
- e. A combination of landscape and Hardscape materials shall be used in the design of the Open Space. Open Space shall be landscaped with trees, as well as turf, shrubs, or groundcover. All plant materials shall be permanently maintained and irrigated and shall be chosen from Appendix G.
 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to use other species that are drought tolerant and adaptive.
- f. In order to achieve a comfortable human scale within Open Spaces and ensure sunlight and air circulation, the following minimum height to width ratios are

required for Open Spaces required by this section:

1. Enclosed Open Space (i.e., Open Space that is enclosed on four sides, such as a courtyard), 2:1 ratio. The required Open Space shall have a width of at least one-half the height of the adjacent building Facade (measured perpendicularly from the Facade). This requirement shall apply to all sides of the required Open Space.
2. Open Space that is open on one or more sides, 3:1 ratio. The required Open Space shall have a width of at least one-third the height of the adjacent building Facade (measured perpendicularly from the Facade). This requirement shall apply to all sides of the required Open Space.



Open Spaces should encourage diverse opportunities for social activities, provide relief and relaxation, expand and reinforce the public realm, and facilitate livability.

(e) Street and Streetscape Standards:

These Standards provide for design of new and improved Streets and Private Vehicular Routes, as well as the treatment of areas between the curb and the private property line and Street medians, if any, including the placement and installation of trees, Sidewalk paving, Street Furnishings, lighting, and other amenities for pedestrians.

- (1) General Street Standards: Streets/Private vehicular Routes shall be built or improved in accordance with this section, Appendix F and the construction standards of the city. The city shall have the ultimate approval authority on Streetscaping on Streets and may establish standards by ordinance for different Street types and conditions. Unless otherwise provided herein, all Right-of-Way dedication and construction for Streets (existing and new) shall be in accordance with the LDR.
- (2) Street Design Standards: Streets shall include a Travel Zone and a Streetscape Zone per Appendix F.

- (3) Private Vehicular Routes: Private Vehicular Routes shall be allowed within existing developed properties in lieu of required Primary Pedestrian Streets designated on the Framework Plans in Exhibit D where existing drives at those locations currently provide internal vehicular circulation. Private Vehicular Routes shall meet all standards for new Streets and design requirements for Primary Pedestrian Streets in Exhibit F.
 1. Administrative Modification: Private Vehicular Routes may be requested in all other locations in lieu of Primary Pedestrian Streets designated on the Framework Plans in Appendix D with a Concept Plan.

- (4) Existing Streets: The Framework Plan in Appendix D depicts existing Streets, and Primary Pedestrian Streets/Private Vehicular Routes within the Corridor District. At such time when properties adjacent to existing Streets develop, and/or redevelop, the property owner shall, for the length of Street frontage of the property, dedicate to the city sufficient Public Right-of-Way (or equivalent) to accommodate the required Street and Streetscape improvements, in accordance with provisions in the LDR and any other applicable ordinances. Where the property does not include a Private Vehicular Route or adjoin a Primary Pedestrian Street, a minimum of one block face per existing block of the improved portion of the property shall be designated as a Primary Pedestrian Street as dictated by existing conditions of surrounding property.
 - a. **Specific Standard for Administrative Modifications:** The Street Design Standards in Appendix F may be adjusted through an Administrative Modification if staff finds that such an adjustment is necessary in order to fit existing and future utility locations, existing landscaping and development, or the International Fire Code as adopted by the city.
 - b. In addition to the requirements of the LDR, the applicant shall be responsible for improvements outside of the Travel Zone in the Public Right-of-Way or Private Vehicular Route or as per any approved agreement with the city.
 1. Fee-In-Lieu Option: An applicant may opt to pay a proportional fee in lieu of the required Streetscape improvements between the curb and the property line if the Development is phased or the Sidewalk improvements need to match the timing of a programmed citycapital project affecting that Street frontage. This fee will be proportional to the linear Street frontage along the subject property. The fee shall be calculated based on construction cost estimates provided to the City for review. Payments shall be made prior to the issuance of any building permit or site permit for the Development.



Illustrations delineating the Streetscape Zone elements and Optional Landscaping along Building Fronts

(5) New Streets:

a. Any new Street shall:

1. Meet the Block Perimeter maximums established in this section;
2. Continue any established Street and block pattern on adjoining sites developed or redeveloped under this chapter;
3. Be designed in accordance with the Street Design Standards in Appendix F; and
4. Designate Primary Pedestrian Streets in compliance with the Framework Plans in Appendix D.

b. For any new Street, the property owner shall construct the entire Street section including travel lanes, On-street Parking, Amenity Zone including irrigation system, and Sidewalk improvements. Half Streets may be provided where necessary to comply with the LDR.

c. Unless otherwise provided in this chapter, all Right-of-Way dedication and construction for Streets (existing and new) shall be in accordance with the LDR.

(6) Streetscape Zone

a. Primary Pedestrian Streets: The Streetscape Zone along new or existing Primary Pedestrian Streets/Private Vehicular Routes shall be a minimum of 12 feet in width. Of this Streetscape Zone, a minimum of six (6) feet shall be designated for a clear Sidewalk and six (6) feet for the Amenity Zone.

b. All other Streets: The Streetscape Zone shall be a minimum of eight (8) feet in

width. Of this Streetscape Zone, a minimum of four (4) feet shall be designated for a clear Sidewalk and four (4) feet for the Amenity Zone unless a greater width is specified in the LDR or the Street Design Standards provided in Appendix F.

- c. Sidewalks shall be located adjacent to the Amenity Zone and shall comply with the requirements as outlined in this section and Appendix F. The sidewalk must be unobstructed by any permanent or nonpermanent element for a minimum height of nine (9) feet. Accessibility is required to connect Sidewalks on adjacent sites.
- d. Enhanced Sidewalks shall be required where recommended in the Lewisville Trails Master Plan.
- e. Sidewalks shall be constructed of concrete or Pavers and may be embellished with earth tones or the same colors and patterns found on the adjacent Facade(s).



Buildings within the Core Sub-district shall be located adjacent to the Sidewalk.

- (7) Trails
 - a. Trails shall be designed and constructed in accordance with the most recently adopted Lewisville Trails Master Plan.
 - b. Any conflicts between the trail requirements and the Sidewalk requirements

herein should be resolved with staff prior to submitting an Engineering Site Plan or Concept Plan.

(8) Street and pedestrian lighting

a. General:

1. All types of exterior lighting shall comply with section 9-3 of the city code, as amended.
2. All lighting shall be directed downward.
 - i. **Specific Standard for Administrative Modification:** Up-lighting of building walls may be approved with an Administrative Modification.
3. Lighting of signage, parking and Walkways is to be controlled or narrowly focused.
4. Placement and shielding of light sources shall be done to limit visual impact. Full cut-off metal fixtures are required.
5. Distracting, flashing, traveling or animated lighting shall not be permitted with the exception of lights associated with seasonal displays.

b. Street lighting: Pedestrian-scaled Street lighting shall be provided along all Sidewalks and Walkways as follows:

1. Maximum height of the light pole shall be 15 feet.
2. Street lights shall be placed at 40' on center, approximately two (2) feet behind the curb line.
3. The light standard selected shall be used consistently within an approved Concept Plan area.

c. Exterior building lighting:

1. Building lighting shall accentuate important architectural components of the building, such as entries, towers or roof elements, or repetitive columns or bays.
2. Building lighting shall provide indirect or direct lighting for adjoining Sidewalks and Open Spaces.
3. Equipment and lighting fixtures shall be weather resistant.
4. Lighting fixtures shall include directional shields so as to prevent viewing of the light source.
5. Building lighting shall be limited to decorative lighting. Standard pack lights may not be attached on buildings except within loading and service areas.
6. LED, halogen, metal halide, incandescent, and compact fluorescent lighting are permitted for exterior lighting.
7. High-intensity discharge (HID) lights are prohibited.
8. **Specific Standard for Administrative Modification:** Neon lighting may be permitted subject to approval of an Administrative Modification.

d. Landscape lighting: Landscape lighting may be used to highlight landscape

elements, building entries and other important architectural features and accent elements such as fountains and sculptures.

(9) Pedestrian amenities

- a. Street Furnishings shall be located fully within the Amenity Zone. In addition to trees and lighting, an applicant shall provide all of the required Street Furnishing elements and at least two (2) of the optional Street Furnishing elements. Street Furnishings shall be required within the Amenity Zone along each block face of a Primary Pedestrian Street/Private Vehicular Route. Street Furnishings shall be optional along all other Streets. Street Furnishings within any Public Right-of-Way shall be maintained by the adjacent property owner.
- b. Required Street Furnishings:
 1. Trash receptacles: shall be constructed of metal (steel or aluminum) with a black polyester powder coat finish and a side opening. Trash receptacles shall be approximately 25 inches in diameter and 34 inches high, with an approximate 28 gallon capacity. They shall be placed at a frequency of two (2) per every 500 linear feet of block frontage.
 2. Bike racks: shall provide at least one (1) bike rack accommodating a minimum of six (6) bicycles per 400 linear feet of block face. An area of two (2) feet by six (6) feet must be provided for each bicycle parking space in order that a bicycle six (6) feet long can be securely held in place with its frame supported by the rack. A bicycle must be able to be placed in the rack in an upright manner that will not damage its wheels or components. The individual floor-mounted “inverted-U” style or “staple” style rack is recommended, but not required.
- c. Optional Street Furnishings:
 1. Planters: If provided, planters shall be constructed of natural grey concrete/sandstone or beige/light tan precast concrete/cast stone, with a drainage hole for irrigation, and shall be of a round tapered shape. Planters may be circular (24” – 42” in diameter) or square/rectangular (24” - 48” wide) with a height between 24 and 36 inches. A minimum of two (2) planters shall be placed for every 200 linear feet of block frontage. Planters shall be irrigated and maintained with landscaping in a healthy growing condition at all times.
 2. Benches: If provided, benches shall be constructed of metal (steel or aluminum) with a black polyester powder coat finish. Bench seats shall have two-seat configurations, with no more than three (3) sets of seats. Seating surfaces shall be 16 to 18 inches high with a minimum depth of 16 inches for seats without backs and 14 inches for seats with backs. They shall be placed at a frequency of one (1) per every 40 linear feet of block frontage.

3. **Bollards:** If used, bollards shall be constructed of metal (steel or aluminum) with a black polyester powder coat. Bollards may have a maximum height of 36 inches and a maximum diameter of nine (9) inches. Bollards shall be placed at a regular spacing of four (4) to six (6) feet on center.
4. **Tree Grates:** If used, they shall be placed around the trees and be manufactured of cast iron with a baked oil finish, and shall be six (6) feet by six (6) feet square, with maximum ½ inch square openings.
5. **Specific Standard for Administrative Modification:** Other optional Street Furnishing elements such as Art, water features, water fountains, newspaper racks, etc., may be proposed and approved through Administrative Modifications.

(f) Screening of Service Areas and Utilities:

These Standards provide for the screening of service and loading areas, roof or ground mounted utility equipment and similar uses on the site.

- (1) **General provisions:** Loading and outdoor storage areas, ground-mounted equipment, roof-mounted equipment, outdoor receptacles and utility accessories shall be screened to reduce the visual impact of these elements on adjoining properties and Public Rights-of-Way. All screening walls shall be measured at the highest finished grade and designed by a professional civil engineer registered in the state. Construction and location details of the required screening devices shall be shown as part of the Engineering Site Plan. In areas where non-single-family Development is proposed adjacent to established single-family residential dwellings and a screening wall is required, the screening wall shall be constructed prior to issuance of a building permit. The screening wall portion of the project costs may not be escrowed under the performance escrow policy as established in the LDR.
- (2) **Loading areas:** Where off-street loading areas for non-residential and Mixed-uses are required by the LDR, such areas shall be screened with one or a combination of the following:
 - a. A minimum eight-foot-tall solid brick, stone, or decorative block Masonry wall, of the same color(s) as the Principal Building on the site;
 - b. Solid metal gates; or
 - c. Overhead doors.
- (3) **Outdoor receptacles,** as defined by city code, shall be enclosed on three (3) sides by solid brick, stone, or decorative block Masonry wall, of earth tones or the same color(s) as the Principal Building on the site. The wall shall be a minimum six (6) feet in height or equal to the height of the equipment to be screened, whichever is greater. All other outdoor receptacle requirements in the city code apply.

- (4) Outdoor storage areas as an accessory use under the Zoning Ordinances shall be located behind a Principal Building and shall be screened from view of Streets/Private Vehicular Routes (excluding alleys) and adjacent properties. Screening walls for outdoor storage shall be either of the following:
 - a. Masonry walls (minimum six (6) feet tall, maximum eight (8) feet tall) of a natural color or the same color(s) as the Principal Building on the site; or
 - b. Tubular steel fence (minimum six (6) feet tall, maximum eight (8) feet tall) with an irrigated, solid landscape screen consisting of evergreen variety trees and/or shrubs maintained at the same height as the steel fence.
 - c. Materials, equipment or commodities shall be stacked no higher than the height of the screen mechanism.

- (5) Roof-mounted equipment: All roof-mounted equipment, including fans, vents, and air conditioning units and cooling towers, shall be screened to eliminate the view of the equipment from any adjoining Streets/Private Vehicular Route (excluding alleys).
 - a. The height of the screening mechanism shall be the height of the tallest element of the roof-mounted equipment. A Parapet or architectural design element on a building may screen roof-mounted equipment; however, the Parapet or architectural design element shall be limited to maximum six (6) feet in height. Roof-mounted equipment taller than six (6) feet shall be screened separately by a solid wall or metal panel/louver system.
 - b. The outside of the screening mechanism shall be painted or finished in a similar color to the building Facade, trim or roof surface and be architecturally integrated with the design of the building elevation.
 - c. The roof-mounted equipment and the inside of the screening device shall be painted similar to the color of the roof surface in order to minimize visibility of the roof-mounted equipment and screening mechanism from overhead views from the adjacent properties.

- (6) Ground-mounted equipment: All ground-mounted mechanical, electrical or other utility equipment shall be located behind the rear line of the building or along alleys without frontage along any Streets/Private Vehicular Route. If an Alternative Standard is approved allowing placement along a Street/Private Vehicular Route frontage, the equipment shall be screened and the screening shall be as tall as the equipment being screened and shall eliminate the view of the equipment from an adjoining Streets (excluding alleys). The screening may be evergreen shrubbery or Masonry walls matching the Principal Building material and color(s) of the site. All screening shall be architecturally integrated into the building elevation and Facade along that Street/Private Vehicular Route.

17.5-5. Transition Sub-District Regulations

(a) Purpose, Intent and Applicability:

- (1) Purpose and intent: The vision for the transition sub-district is to create an appropriate transition of the development context from the core sub-districts to existing neighborhoods by:
 - a. Maintaining auto-oriented uses while softening their frontages along major roadways with landscaping and Facade improvements;
 - b. Focusing on creating a more attractive frontage along the IH-35E highway corridor;
 - c. Minimizing visual clutter with respect to Driveways, signage, lack of landscaping, and older declining building Facades; and
 - d. Establishing a long-term Redevelopment strategy to elevate property values and development context.

- (2) Applicability. The regulations in this section, as illustrated in Table 1, apply to any property within the transition sub-district as shown on Exhibits 1 through 5 in section 17.5-3.

(b) Building and Envelope Standards:

These Standards establish where the building should be placed on the property with respect to setbacks and build-to lines, the orientation of a building’s Facades, alignment of new Streets and blocks, height of buildings, Driveways and parking, and pedestrian accommodations on the site.

- (1) Building placement:
 Building setbacks shall be measured from the property line and shall be provided in accordance with the requirements in Table 3 of this section.

Table 3. Building Placement Standards		
Street Frontage	Building Setbacks	
	Minimum (feet)	Maximum (feet)
Front (by Street Type)		
IH-35E Frontage Road	20	None
Arterial roadway	20	None
Collector or Local Street (2-4 lanes)	10	None
Other or Alley	5	None
Side (interior)	0 *	None
Rear (non-alley)	0 *	None

*Except that any setbacks adjacent to single family residential uses located outside the Corridor District shall be a minimum of 20 feet.

- (2) Building orientation: If the building only has one Street frontage on the IH-35E Frontage Road or an arterial roadway, then the longer side of the building shall be placed parallel to the Street. For all properties which are adjacent to the IH-35E Frontage Road or an arterial roadway, the front Facade of the building shall be oriented to the higher category roadway.

- (3) Blocks and lots:
 - a. Maximum Block Perimeter shall be 2,400 feet.
 - b. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow for a 20% increase to the maximum Block Perimeter requirement.

- (4) Building Height:
 - a. The minimum Building Height of a building or Parking Structure shall be 20 feet.
 - b. Minimum interior Ground Floor height shall be 12 feet (clear).

- (5) Driveways:
 - a. Sharing and consolidation of Driveways is encouraged in order to reduce the number of curb cuts to ensure smoother, more organized traffic movements and less disruption of pedestrian movement.
 - b. Cross or joint access easements:
 - 1. Cross or joint access easements shall be required for all Developments unless grade and topography make such cross access unfeasible.
 - 2. Cross or joint access easements shall be designated at the time of Development in anticipation of future direct or indirect access.
 - c. Except where otherwise required by the Access Management Policy in the LDR, curb cuts (Driveway openings) shall:
 - 1. Be a minimum of 300 feet apart from any other curb cut or Street intersection; and
 - 2. Not interrupt the paving material of the Sidewalk with another material. Sidewalk paving shall be continuous across the Driveway.
 - d. Driveway entrances into multi-building commercial properties along the IH-35E Frontage Road and all arterial roadways shall be enhanced with the addition of signs, accent paving, special landscaping and/or lighting. Design elements shall not be located within any required visibility easements or clear vision triangles.

- (6) Pedestrian circulation
 - a. Sidewalks

1. Sidewalks for separate and continuous pedestrian circulation shall be provided along all Streets.
 - b. Walkways
 1. Walkways shall be no less than four (4) feet in width.
 2. Walkway connections shall be required:
 - i. Between any existing or future Sidewalk, trails, parks or greenways and Primary Entrances of all buildings on the site;
 - ii. From building entrances to all on-site facilities, such as Parking Lots, bicycle facilities, and Open Space;
 - iii. Up to the property line in a manner which will allow walkway connections between adjoining properties and buildings and
 - iv. To connect to any public transit stop that is adjacent to a site.
 3. Walkways shall be distinguished from any driving surfaces through the use of colored pavements, bollards, grade changes, pavement markings or combination of treatments, especially when Walkways cross or are adjacent to vehicular circulation areas.
 - c. Parking
 1. Parking Lots and Parking Structures shall clearly mark Walkways between parked vehicles and Primary Entrances through the use of clearly marked stop signs, wayfinding/directional signage, lighting, and other similar measures.
 2. Parking stalls shall not overhang onto any Walkway, Sidewalk or landscape area and shall require wheel stops or an increase in the minimum Walkway, Sidewalk or landscape area of at least the depth of the overhang.
- (7) Off-street Parking
- a. On-site parking shall be provided in accordance with the following parking requirements:
 1. Non-residential uses: one (1) space per 250 square feet of gross floor area.
 2. Residential uses: two (2) spaces per residential unit.
 3. Mixed-uses: use ratios above to calculate the required spaces based on the composition of the uses on the property.
 - b. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow credit for available public parking or shared Off-street Parking on another lot within 1,000 feet of the subject property to apply towards On-site Parking requirements.
 - c. Shared Parking Lots shall be connected to businesses with paved and landscaped Walkways per subsection 17.5-5(b)(6), above.
- (8) On-street Parking is encouraged, but not required, along all collector and Local Streets.

(c) Architectural Standards:

These Standards apply to the exterior architectural treatments within the transition sub-districts. External architectural treatments include building Articulations, Facade composition, exterior materials and colors, windows and doors, and Awnings and Canopies. These Standards are intended to elevate the visual appeal of the Corridor District through a common set of minimum building design Standards.

(1) Building Articulation and Facades

- a. Building Facades visible from any Street (except alleys) or Open Space shall demonstrate horizontal and vertical Articulation with a Facade Rhythm between twenty to forty feet. This rhythm may be expressed by any of the following:
 - 1. a change in a building's horizontal or vertical plane;
 - 2. stepping portions of Facades in and out;
 - 3. utilizing balconies, columns or pilasters that are distinctively set out from the Facade; or
 - 4. changing types or colors of materials in combination with other techniques.



Examples of attractive exterior architectural treatment

- b. All building Facades along IH-35E Frontage Road or an arterial roadway shall have a minimum of 40% the Ground Floor Facade comprised of windows and doors.
- (2) Building entrances:**
- a. Primary Entrances shall be emphasized architecturally with Awnings, recessed entries, pilasters, etc.
 - b. A minimum twelve-foot wide Secondary Walkway shall be required along each building's front Facade that is comprised of the Primary Entrances into businesses and/or tenant spaces, if such a Facade faces a Parking Lot on the property. This Secondary Walkway shall incorporate shading elements along its entire front Facade in the form of Canopies, trees, or a combination of the two.
 - 1. Canopies may be used for no more than 50% of the Facade length and shall

extend at least two (2) feet from the Facade.

2. Trees shall be planted at an average spacing of 40 feet. Trees shall be selected from the Canopy Tree List in Appendix G and shall be a minimum of two and a half (2¹/₂) inch caliper when planted.
 - i. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to use other species that are drought tolerant and adaptive.



Illustration depicting the Secondary Walkway requirement along the interior storefront

(3) Facade materials and colors

- a. Facade materials: Each exterior wall (except window and door area) of a building or Parking Structure shall comply with the following material requirements:
 1. Eighty percent (80%) or more of each exterior wall shall be comprised of brick or stone. The remainder of each exterior wall may be comprised of either three-step stucco (EIFS shall only be allowed at locations nine (9) feet above grade), architectural metal, cladding, or panels, or a combination thereof.
 2. Accent materials may be comprised of pre-cast stone, metal, or other architectural materials.
- b. Colors: At least two (2) main colors, and no more than three (3) colors, shall be used on each elevation. Eighty percent (80%) of each elevation must utilize natural and/or earth tone colors, and each elevation must have 20% of another type of color. Buildings shall not have colors that are fluorescent or bright (e.g. yellow, green, purple) visible from any public place or Street. This shall include service doors, down spouts, utility boxes, panels and other similar features integral to the Principal Building.



Building Facade with no more than two colors with 80 percent natural/ earth tone colors.

(4) Awnings and Canopies

- a. All building fronts with business/tenant space entrances shall have an Awning or Canopy which extends beyond the face of the building over an adjacent Walkway or Sidewalk for a minimum of 50% of the building's front Facade width.
- b. Materials allowed shall consist of metal or canvas. Reflective materials for finishes are prohibited.
- c. When used, Awnings or Canopies shall be located as to provide shading for exterior windows and doors and shall cover the entire width of the window opening or groups of windows over which it is installed.
- d. Awnings and Canopies may encroach over Walkways up to 50% of the width of the Walkway or five (5) feet, whichever is greater. They shall maintain a minimum nine-foot (9') vertical clearance as measured between the bottom of the Awning/Canopy and the finished grade of the Walkway. In no case shall an Awning or Canopy encroach over a Travel Zone, Driveway or fire lane.



Awnings should fit into the storefront's overall architecture and appearance. Awnings provide protection and cover from bright sunlight and inclement weather.

(5) Windows

- a. Window framing materials shall consist of anodized aluminum, vinyl, or steel.
- b. For required windows at the Ground Floor, a minimum 60% visible light transmittance shall be required.
 - 1. **Specific Standard and Findings for Administrative Modifications:**
Windows having a visible light transmittance less than 60% may be allowed with an Administrative Modification if a finding is made by staff that an Administrative Modification is necessary to satisfy building code requirements related to energy efficiency.
- c. Mirrored glass is prohibited.
- d. No more than 50% of the windows on any Facade may be Reflective Glass.
- e. Window screens are prohibited on non-residential Ground Floors.
- f. Horizontal groupings of windows shall not exceed five (5) per grouping, where groupings are separated by a mullion, column, or wall section a minimum of seven-inches wide.
- g. Windows shall be a minimum of thirty inches from building corners, unless separated by a corner mullion or column twice the width used in grouping.
- h. The following are prohibited for use in storefronts:
 - 1. Single pane glass windows;
 - 2. Black glass, opaque glass, and other ‘false window’ techniques;
 - 3. Doors with no opacity; and
 - 4. Windows which do not permit unobstructed views into the building.



Windows should maximize visibility to the Street on the Ground Floor. Ensure that commercial Ground Floor uses provide clear unobstructed windows, free of reflective coatings.

(6) Parking Structures

- a. Parking Structures shall have a Facade design with the same colors and materials as the building the Parking Structure Services. If the Parking Structure serves multiple buildings, it shall have a Facade design with the same colors and materials. Parking Structures shall be located behind buildings to minimize their visibility from adjacent Streets.

- b. Where a Parking Structure is located adjacent to any Street:
 - 1. Parking Structure Facades shall be designed with both vertical Articulation at least every 40 linear feet and horizontal Articulation aligning with horizontal elements along the block. The building materials shall be the same materials and color(s) as the building the Parking Structure serves.
 - 2. The Parking Structure shall be designed and screened in such a way that motor vehicles on all parking levels are hidden from view from all adjacent Streets. Parking Structure ramps shall not be visible from any Street. Ramps shall not be located on the perimeter of the Parking Structure. Architectural Screens shall be used to articulate the Facade, hide parked vehicles, and shield the lighting inside the structure.



Parking Structures shall be located behind buildings to minimize their visibility from adjacent Streets.

Parking Structures shall have a Facade design similar to surrounding buildings along the Street front.

(d) Landscape Standards:

These Standards apply to new trees and shrubs, Hardscape and Open Space treatments, screening of service, parking and utilitarian uses, and Parking Lot landscaping. Landscaping can reduce the urban heat island effect, soften the built environment and contribute to pedestrian comfort in addition to increasing property values and rents.

(1) Trees and plant materials

- a. Plantings may be placed in natural groupings along Street frontages.
- b. Tree and plant materials shall be selected by the landscape architect or designer designing the landscape plan from the approved plant material list provided in Appendix G. No artificial trees, shrubs, ground covers, turf, or seasonal colors are permitted.

- 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to use other species that are drought tolerant and adaptive.

- c. Irrigation systems provided within all landscaped areas including the Secondary Walkway area and shall be installed and maintained by the developer or property owner per current Parks and Recreation Department standards.
- d. Canopy trees shall be:
 - 1. Planted along Street frontages within the Amenity Zone and/or Secondary Walkways;
 - 2. Planted at an average spacing of forty feet on center along all Street frontages; and
 - 3. A minimum three (3) inch caliper and at least ten (10) feet in height with a single trunk at planting.
- e. Tree preservation and landscape maintenance shall comply with the provisions in the LDR.
- f. Parking Lots with frontage on IH-35E shall provide a minimum twenty-foot wide landscape area between the Public ROW and Parking Lot. Canopy trees shall be placed at an average spacing of forty feet. A solid evergreen shrubbery hedge with a minimum 24-inch height at the time of planting shall be planted in a manner which will screen the view of parked vehicles from the Street.
 - 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to allow any of the following (minimum height of 36-inches) as a Parking Lot screen:
 - i. Retaining wall;
 - ii. Berming; or
 - iii. A combination of retaining wall, berming, and/or evergreen screening hedge.
- g. Parking Lots adjacent along all other Street frontages (except alleys) shall use one or both of the following screening methods:
 - 1. A minimum five-foot wide landscape area between the Sidewalk and Parking Lot, with a solid evergreen shrubbery hedge with a minimum 24-inch in height at the time of planting planted within the landscape area in a manner which will screen the view of parked vehicles from the Street; or
 - 2. A Masonry (brick or stone) wall a minimum of 36-inches in height installed between the Sidewalk and the Parking Lot, placed such that a minimum two-foot parking space overhang is provided on the Parking Lot side of the wall, and wheel stops are provided for the parking spaces.
- h. All Parking Lots shall include the following landscaping elements:
 - 1. Expanses of parking in excess of 120 parking spaces shall be organized into blocks of parking with no more than 120 spaces in each block. Each block of parking shall be separated from other parking blocks by a minimum of one (1) ten-foot landscape island with plant cover and trees as required below, as well as Walkways of at least four (4) feet in width leading to the front Facade of the building. Driveways between the parking blocks shall be provided.

2. Landscape islands shall be a minimum of ten (10) feet in width with a minimum of 50% plant cover. A landscape island shall be required on the end of each row of parking in addition to between each parking block.
3. Each Parking Lot shall include canopy trees at a ratio of one (1) tree per eight (8) parking spaces.

(2) Hardscaping

- a. All hardscaping materials shall be of earth tones or colors found on the Principal Building.
- b. Walkway and Secondary Walkway paving shall be comprised of the following materials or a combination thereof:
 1. Concrete;
 2. Pavers; or
 3. Stamped concrete or asphalt.

(3) Open Spaces

- a. Open Space shall be provided on Developments that incorporate five (5) acres or more.
 1. The minimum set aside for Open Space shall be 5% of the Development.
 2. If the Open Space is designed for public use, it shall be highly visible from the Public ROW and accessible to the general public. If the Open Space is designed solely for private use, it shall be centrally located and easily accessible to all individuals it is expected to serve.
- b. Developments that incorporate less than five (5) acres may incorporate Open Space for the use of employees or the public.
- c. Private Open Space may be fenced or otherwise controlled for secure access using wrought iron, Masonry, or comparable decorative fencing, or vegetative screening.
- d. Parking, Driveway, or rear setback areas may not be used as publicly accessible Open Space, but publicly accessible Open Space areas may include front and side setback areas provided that they are integrated into the overall design of the project. Publicly accessible Open Space shall have public or private street frontage for a minimum of 25% of the perimeter of the Open Space. Retention and detention areas that are incorporated into the overall site design with appropriate amenities may be credited towards 50% of the Open Space requirement.
- e. A combination of landscape and Hardscape materials shall be used in the design of Open Space. Open Space shall be landscaped with trees, as well as turf, shrubs, or groundcover. All plant materials shall be selected from Appendix G and permanently maintained and irrigated.
 1. **Specific Standard for Administrative Modification:** An Administrative Modification may be requested to use other species that are drought tolerant

and adaptive.



Open Spaces should encourage diverse opportunities for social activities, provide relief and relaxation, expand and reinforce the public realm, and facilitate livability.

(e) Street and Streetscape Standards:

These Standards provide for design of new and improved Streets as well as the treatment of areas between the curb and the private property line and Street medians, if any, including the placement or installation of trees, Sidewalk paving, Street Furnishings, lighting, and other amenities for pedestrians.

- (1) General Street Standards: Streets shall be built or improved in accordance to this section, the Street Design Standards in Appendix F, and the construction standards of the city. The city shall have the ultimate approval authority on Streetscaping on Streets and may establish standards by ordinance for different Street types and conditions. Unless otherwise provided herein, all Public ROW dedication and construction for Streets (existing and new) shall be in accordance with the LDR.
- (2) Street Design Standards: Streets shall include a Travel Zone and a Streetscape Zone per Appendix F.
- (3) Existing Streets: At such time when properties adjacent to the existing Streets develop, and/or redevelop, the property owner shall, in accordance this section and for the length of Street frontage of the property, dedicate to the city sufficient Public Right-of-Way (or equivalent) to accommodate the required Street and Streetscape improvements, subject to provisions in the LDR and any other applicable city ordinances.
 - a. **Specific Finding for Administrative Modification:** The Street Design Standards in Appendix F may be adjusted through an Administrative Modification if staff finds that such an adjustment is necessary in order to fit existing and future utility locations, existing landscaping, or the International Fire Code as adopted by the city.

- (4) New Streets:
 - a. Any new Street shall:
 - 1. Meet the Block Perimeter maximums established in this section;
 - 2. Continue any established Street and block pattern on adjoining sites developed or redeveloped under this chapter; and
 - 3. Be designed in accordance with the Street Design Standards in Appendix F.
 - b. For any new Street, the property owner shall construct the entire Street section including travel lanes, On-street Parking, and any other Sidewalk improvements. Half Streets may be provided where necessary to comply with the LDR.
 - c. Unless otherwise provided in this chapter, all Right-of-Way dedication and construction for Streets (existing and new) shall be in accordance with the LDR.

- (5) Streetscape Zone
 - a. The Streetscape Zone along all streets shall be a minimum of nine (9) feet in width. Of this streetscape Zone, a minimum of five (5) feet shall be designated for a clear Sidewalk and four (4) feet for the Amenity Zone.
 - b. Sidewalks shall be located adjacent to the Amenity Zone and shall comply with the requirements as outlined in this section and Appendix F. The sidewalk must be unobstructed by any permanent or nonpermanent element for a minimum height of nine (9) feet. Accessibility is required to connect Sidewalks on adjacent sites.
 - c. Enhanced Sidewalks shall be required where recommended in the Lewisville Trails Master Plan.
 - d. Sidewalks shall be constructed of concrete or Pavers and may be embellished with the same colors and patterns found on the adjacent Facade(s).

- (6) Trails
 - a. Trails shall be designed and constructed in accordance with the most recently adopted Lewisville Trails Master Plan.
 - b. Any conflicts between the trail requirements and the Sidewalk requirements herein should be resolved with staff prior to submitting any Engineering Site Plan or Concept Plan.

- (7) Street and pedestrian lighting
 - a. General:
 - 1. All types of exterior lighting shall comply with section 9-3 of the city code, as amended.
 - 2. All lighting shall be directed downward.
 - i. **Specific Standard for Administrative Modification:** Up-lighting of building walls may be approved with an Administrative Modification.
 - 3. Lighting of signage, parking, Sidewalks and Walkways is to be controlled or

narrowly focused.

4. Placement and shielding of light sources shall be done to limit visual impact. Full cut-off metal fixtures are required.
 5. Distracting, flashing, traveling, or animated lighting shall not be permitted with the exception of lights associated with seasonal displays.
- b. Street Lighting: Pedestrian-scaled Street lighting shall be provided along all Secondary Walkways, Walkways, and Sidewalks as follows:
1. Maximum height of the light pole shall be 15 feet.
 2. Street lights shall be placed at 40' on center, approximately two (2) feet behind the curb line.
 3. The light standard selected shall be compatible with the design of the Street Furnishings and buildings or per city standards.
- c. Exterior building lighting:
1. Building lighting shall accentuate important architectural components of the building, such as entries, towers or roof elements, or repetitive columns or bays.
 2. Building lighting shall provide indirect or direct lighting for adjoining Sidewalks and Open Spaces.
 3. Equipment and lighting fixtures shall be weather resistant.
 4. Lighting fixtures shall include directional shields so as to prevent viewing of the light source.
 5. Building lighting shall be limited to decorative lighting. Standard pack lights may not be attached on buildings except within loading and service areas.
 6. LED, halogen, metal halide, incandescent, and compact fluorescent lighting are permitted for exterior lighting.
 7. High-intensity discharge (HID) lights are prohibited.
 8. **Specific Standard for Administrative Modification:** Neon lighting may be permitted subject to approval of an Administrative Modification.
- d. Primary Entrance lighting: Primary Entrances shall be externally lit so as to promote a more secure environment at the door, emphasize the primary point of entry into the building, and provide sufficient lighting for efficient access into the building. Standard security lighting, including but not limited to wall-packs, shall only be allowed in service and loading areas.
- e. Landscape lighting: Landscape lighting may be used to highlight landscape elements, building entries and other important architectural features and accent elements such as fountains and sculptures.

(8) Pedestrian amenities

- a. General:
 1. Street Furnishings shall be located within Secondary Walkway areas as

designated in this section so long as a clear area of at least four (4) feet is designated. A minimum of two (2) of the Street Furnishings listed in subsection b. below shall be provided within a Secondary Walkway area. The Street Furnishings shall be shown on an approved Engineering Site Plan and landscape plan prior to installation.

2. Street Furnishings and any adjacent area required for the functional use of the Street Furnishings shall not be within the clear Sidewalk area required by this section.
 3. All Street Furnishings shall be affixed to the ground so as to prevent removal or tipping.
- b. Street Furnishings List:
1. Trash receptacles: shall be constructed of metal (steel or aluminum) with a black polyester powder coat finish and a side opening. Trash receptacles shall be approximately 25 inches in diameter and 34 inches high, with an approximate 28-gallon capacity. They shall be placed at a frequency of two (2) per every 500 linear feet of block frontage.
 2. Bike racks: shall provide at least one (1) bike rack accommodating a minimum of six (6) bicycles per 400 linear feet of block face. An area of two (2) feet by six (6) feet must be provided for each bicycle parking space in order that a bicycle six (6) feet long can be securely held in place with its frame supported by the rack. A bicycle must be able to be placed in the rack in an upright manner that will not damage its wheels or components. The individual floor-mounted “inverted-U” style or “staple” style rack is recommended, but not required.
 3. Planters: shall be constructed of natural grey concrete/sandstone or beige/light tan precast concrete/cast stone, with a drainage hole for irrigation, and shall be of a round tapered shape. Planters may be circular (24” – 42” in diameter) or square/rectangular (24” - 48” wide) with a height between 24 and 36 inches. A minimum of two (2) planters shall be placed for every 200 linear feet of block frontage. Planters shall be irrigated and maintained with landscaping in a healthy growing condition at all times.
 4. Benches: shall be constructed of metal (steel or aluminum) with a black polyester powder coat finish. Bench seats shall have two-seat configurations, with no more than three (3) sets of seats. Seating surfaces shall be 16 to 18 inches high with a minimum depth of 16 inches for seats without backs and 14 inches for seats with backs. They shall be placed at a frequency of one (1) per every 40 linear feet of block frontage.
 5. Bollards: shall be constructed of metal (steel or aluminum) with a black polyester powder coat. Bollards may have a maximum height of 36 inches and a maximum diameter of nine (9) inches. Bollards shall be placed at a

regular spacing of four (4) to six (6) feet on center.

6. Tree Grates: shall be placed around the trees and be manufactured of cast iron with a baked oil finish, and shall be six (6) feet by six (6) feet square, with maximum ½ inch square openings.
7. **Specific Standard for Administrative Modification:** Other Street Furnishing elements such as Art, water features, water fountains, newspaper racks, etc., may be proposed and approved through Administrative Modifications.

(f) Screening of Service Areas and Utilities:

These Standards provide for the screening of service and loading areas, roof or ground mounted utility equipment and similar uses on the site.

- (1) General provisions: Loading and outdoor storage areas, ground-mounted equipment, roof-mounted equipment, outdoor receptacles and utility accessories shall be screened to reduce the visual impact of these elements on adjoining properties and Public Rights-of-Way. All screening walls shall be measured at the highest finished grade, and designed by a professional civil engineer registered in the state. Construction and location details of the required screening devices shall be shown as part of the Engineering Site Plan. In areas where non-single-family Development is proposed adjacent to established single-family residential dwellings and a screening wall is required, the screening wall shall be constructed prior to issuance of a building permit. The screening wall portion of the project costs may not be escrowed under the performance escrow policy of the LDR.
- (2) Loading Areas: Where off-street loading areas for non-residential and Mixed-uses are required by the LDR, such areas shall be screened with one or a combination of the following:
 - a. A minimum eight-foot-tall solid brick, stone, or decorative block Masonry wall, of the same color(s) as the Principal Building on the site;
 - b. Solid metal gates; or
 - c. Overhead doors.
- (3) Outdoor receptacles, as defined by the LDR, shall be enclosed on three (3) sides by a solid brick, stone, or decorative block Masonry wall, of earth tones or the same color(s) as the Principal Building on the site. The wall shall be a minimum six (6) feet in height or equal to the height of the equipment to be screened, whichever is greater. All other outdoor receptacle requirements in the city code apply.
- (4) Outdoor storage areas as an accessory use under the Zoning Ordinances shall be located behind a Principal Building, and shall be screened from view of Streets (excluding

alleys) and adjacent properties. Screening walls for outdoor storage shall be either of the following:

- a. Masonry walls (minimum six (6) feet tall, maximum eight (8) feet tall) of the same color(s) as the Principal Building on the site; or
 - b. Tubular steel fence (minimum six (6) feet tall, maximum eight (8) feet tall) with an irrigated, solid landscape screen consisting of evergreen variety trees and/or shrubs maintained at the same height as the steel fence.
 - c. Materials, equipment or commodities shall be stacked no higher than the height of the screen mechanism.
- (5) Roof-mounted equipment: All roof-mounted equipment, including fans, vents, and air conditioning units and cooling towers, shall be screened to eliminate the view of the equipment from any adjoining Streets (excluding alleys).
- a. The height of the screening mechanism shall be the height of the tallest element of the roof-mounted equipment. A Parapet or architectural design element on a building may screen roof-mounted equipment; however, the Parapet or architectural design element shall be limited to maximum six (6) feet in height. Roof-mounted equipment taller than six (6) feet shall be screened separately by a solid wall or metal panel/louver system.
 - b. The outside of the screening mechanism shall be painted or finished in a similar color to the building Facade, trim or roof surface and be architecturally integrated with the design of the building elevation.
 - c. The roof-mounted equipment and the inside of the screening device shall be painted similar to the color of the roof surface in order to minimize visibility of the equipment and screening mechanism from overhead views from the adjacent properties.
- (6) Ground-mounted equipment: All ground-mounted mechanical, electrical or other utility equipment shall be located behind the rear line of the building or along alleys without frontage along any Streets. If an Alternative Standard is approved allowing placement along a Street frontage, the equipment shall be screened and the screening shall be as tall as the equipment being screened and shall eliminate the view of the equipment from any adjoining Streets (excluding alleys). The screening may be evergreen shrubbery or Masonry walls matching the Principal Building material and color(s) of the site. All screening shall be architecturally integrated into the building elevation and Facade along that Street.

(g) Pad Site Standards:

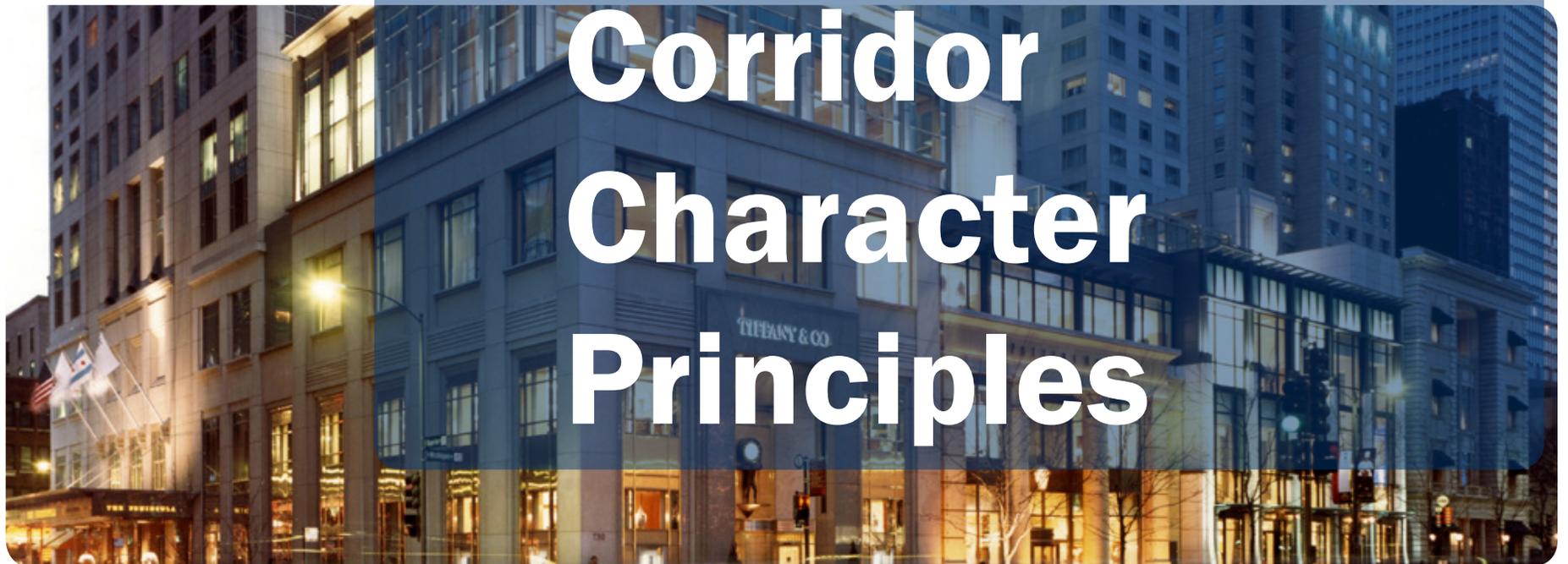
These Standards provide for the development of Pad Sites including the architectural treatment of Facades, continuity of landscaping, screening of drive-through and service functions, and pedestrian accommodation.

- (1) Standards: In addition to all other Standards applicable to Development within the transition sub-district, the following shall apply to Pad Sites:

- (2) Building design and orientation standards:
 - a. All building Facades shall be designed as the front Facade in compliance with all requirements of this section for front Facades. If windows and doors cannot be incorporated into the rear and side Facades, changes of building materials and color shall be used to create the appearance of solids and voids.
 - b. The longer side of the building shall be oriented towards the primary Street frontage and shall be considered the front of the building.
 - c. The Primary Entrance to the building shall be located in the front of the building and shall be oriented toward the primary Street and the Sidewalk.
 - d. The Primary Entrance shall be emphasized along the front Facade with doors, display windows, Canopies, Awnings, increased architectural elements, increased landscaping, lighting, and/or pedestrian-oriented features.
 - e. Pedestrian access shall be provided leading from the Primary Entrance at the Secondary Walkway along the storefront to Sidewalk at the Street.
 - f. Stacking and drive-through lanes shall not be located along the primary Facade of the building.

Appendices

Appendix A
Corridor Character Principles



Corridor Character Principles

Corridor Character Principles

The following corridor character principles are intended to provide a thematic vision for public infrastructure along the corridor and its subareas. This section is organized to present intent statements for spatial elements contained within the corridor, followed by design principles for each of the character subareas. The principles should be used to provide guidance during the decision-making process when evaluating detailed concepts for proposed treatments within the corridor.

These principles were used in the development of the IH-35E Corridor Identity Plan, which adopted the three character zone designations and determined the appropriate limits for each within the corridor. The physical design elements for the corridor were then developed in terms of the aesthetic character which was then applied to typical interchange locations along the corridor. A vocabulary of common elements was established for the entire corridor, including lighting, banner poles, wayfinding, plantings, cross street gateway identifiers, pedestrian protection barriers, decorative crosswalk pavement and bridge abutment wall treatments. Unique character zone defining elements were developed for pedestrian paving patterns, custom retaining wall graphics, pedestrian protection wall forms and textures, and planting palette.

The Corridor Experience

The eight-mile drive along the IH-35E corridor through Lewisville provides a range of experiences that can be interpreted in the physical design of the spatial elements throughout the corridor. This corridor contains modulation through topography, compression of the right-of-way in areas where the highway is depressed, and openness as the highway crests to expansive views of the region. On a smaller scale, the corridor passes under bridges which will serve as landmarks, and along a variety of walled conditions. Through the journey, the corridor also passes along watercourses and open spaces that are amenities to local neighborhoods. These experiences provide inspiration that can inform the design of elements at the scale of the entire corridor and at smaller neighborhood scales.



The eight-mile corridor contains unique character areas including water, urban and park.

Unifying Character Theme

One of the most impressive natural resources along this corridor is Lake Lewisville. The lake is a psychological and physical place of respite and recreation, and is a regional attraction in the Dallas metropolitan area. During work sessions with the project's Advisory Committee, the lake was identified as the most recognizable asset to the city and the theme of water was proposed as a unifying theme for the corridor. The purpose of providing a unifying theme is to provide a consistent brand for the city and to inform drivers of the range of experiences and opportunities contained in this corridor.

Subarea Character Themes

Due to the length of the corridor, the range of experiences it offers, and the character of its adjacent communities, subarea themes are proposed for the north, central and south portions of the corridor. Subarea themes will help to break down scale of the corridor and provide neighborhood identification.



Water should be a defining characteristic in the north section and used as a unifying theme.

North-Water

The subarea themes for the north section of the corridor are water and recreation. Combined with the vacant parcels available for redevelopment along the corridor, the water theme could serve as an organizing element for future development. Lifestyle oriented development could reflect the unique attributes of living, working and recreating around water. The design elements along this northern reach need to come together to draw visitors off the highway thereby capturing greater market share.

- Gateways and landmarks should be focused at Garden Ridge Boulevard and Valley Ridge Boulevard, both primary entrances into the Lake area. Higher density nodal development should be focused around these gateways to draw in visitors.
- Water should be a defining characteristic in the treatment of walls, bridges, lighting and art. Species such as Sun Fish and Large Mouth Bass could be utilized as relief along retaining walls to remind visitors of the Lake Lewisville recreational amenity.

Corridor Character Principles



The central section of the corridor is tied to Old Town Lewisville



Hebron Parkway could benefit from a gateway in the southern section

Central-Urban

The theme of the Central section of the corridor ties to historic Old Town Lewisville. Though located approximately one mile from the center of Old Town, IH-35E currently has little relationship to Old Town and the medical uses in between. Design elements should be consistent with the historic architecture of Old Town.

- Gateways and landmarks need to be located at both South Mill Street and West Main Street to signify entrance into the historic heart of the community, tying newer land uses along the highway with the history of Old Town.
- Design treatments for walls, bridges, lighting, building architecture and art should emphasize the area's history. Brick and warm materials need to reflect the historic nature of Old Town.

South-Park

The southern section of the IH-35E corridor will remain an important center of retail, hospitality and corporate office space. The theme for this area should focus on its location as a gateway and destination, and capitalize on its unique assets, such as Timber Creek.

- A gateway is needed at Hebron Parkway to distinguish this area as a center of activity and connectivity between uses east and west of the highway. This bridge could exhibit a unique architectural style, such as an arched truss bridge. Tall lighted elements along this bridge could denote this as a special area, and create a nighttime place, while preserving views north and south during the day.
- Due to the complexity of the intersection of SH-121 to allow for all necessary vehicle movements, this area will contain a high percentage of retaining walls. Special attention will be required to reduce the impact of the walls through a variety of surface treatments, including massing breaks, color changes, and texture variations. Additional surface treatments could include the incorporation of public art to display shallow relief images with colored material.

Unifying character theme spatial elements

The experience and character of the corridor will be communicated through the design and treatment of its spatial elements. Following are design principles for the primary spatial elements that will be experienced throughout the corridor. Each of the spatial elements described below should be designed to complement the unifying theme and respective subarea themes along the IH-35E corridor.

Each spatial element is defined below. Intent statements describe how each element is characterized in the corridor. Principles are included that should be incorporated in the design of each element.

A. Landmarks and gateways take the form of natural features and design elements and include bridges, intersection treatments, development form, and significant open space areas.

Intents:

- Landmarks identify and brand the City of Lewisville and its neighborhoods.
- Landmarks form a mental map for wayfinding purposes and can be created through natural or manmade means.
- Gateways identify particular areas within the city or particular neighborhoods.



Landmarks can enhance wayfinding



Gateways should be at least 8 feet tall

Principles:

- A1.** To achieve a consistent treatment where gateways are provided, gateways should be designed as elements that either span across a highway or path, or are symmetrical treatments located on each side of the highway right-of-way or path, and require users to either pass under or pass through.
- A2.** To promote the diversity that exists in Lewisville, landmark locations are identified within the primary catalysts of Subareas 2, 5 and 7.
- A3.** To diversify community image, landmark and gateway treatments should contain a minimum of three (3) exterior materials.
- A4.** To promote visual clarity, gateway treatments should be a minimum of eight-feet (8') tall at any dimension from the ground or base condition.
- A5.** To incorporate nodal development, landmarks and gateway treatments should be integrated with intersection design at identified nodes.

Corridor Character Principles

B. Development Patterns describe how buildings, roads and open space are organized together in a particular area. These patterns contribute significantly to the character of the highway.

Intents:

- Development is composed of compact centers, or nodes, of mixed use development.
- Environments are designed to be comfortable to pedestrians and bicyclists.
- Networks of transportation modes interconnect development and open space.
- Land use and transportation are intricately linked to create active, engaging places.

Principles:

B1. To reduce sprawl, create development nodes that capture a higher number of vehicle trips accessing the corridor than traditional development.

B2. To promote active places, create centers of mixed-use developments near a variety of residential densities.

B3. To promote active places, site design for major projects should allow for increased densities over time.

B4. To reduce the occurrence of strip development, new development should be nodal in character and concentrate development along the highway at planned areas that are separated by open space.

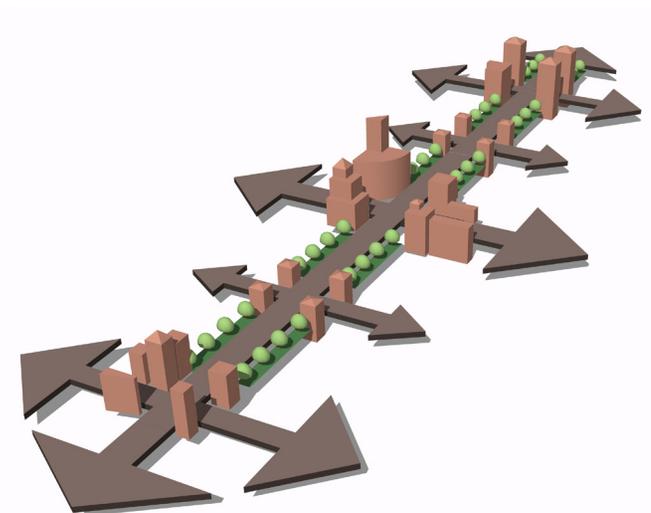
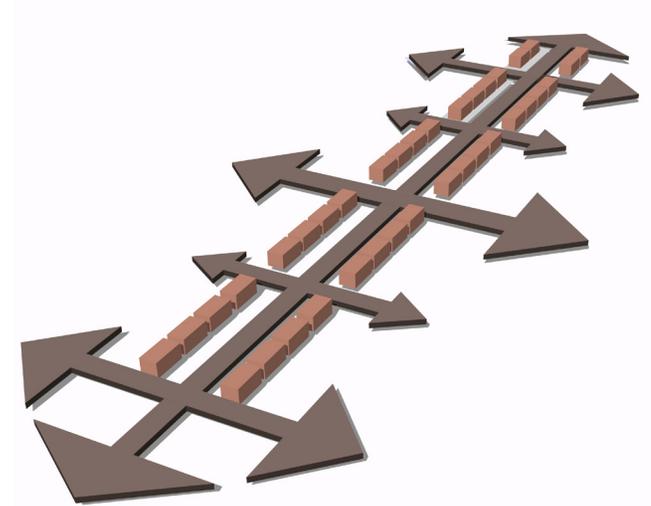
B5. To leverage transportation access, the tallest and densest development patterns should occur within 700 feet to one-quarter mile radius of planned nodes. Most nodes are situated at interchanges.

B6. To improve development character, parking locations should be less prominent and located to the rear of buildings or in parking structures.

B7. To promote active streets, pedestrian-oriented uses should be located on ground floors of buildings.

B8. To facilitate more active places, sidewalks should be wider in planned development nodes than in other lower density areas.

B9. To provide amenities for pedestrians, sidewalks should incorporate street trees, benches, kiosks and plazas.



Low-density development (top) and node development (bottom)

B10. To promote active streets, auto-oriented uses, including service stations and drive through facilities should be discouraged within one-quarter mile radius of planned nodes.

B11. To provide a pedestrian-friendly street network, street block sizes should not exceed 600,000 square feet.

C. Right-of-way treatments include bridges, walls, fencing, landscaping and lighting.



Intents:

- Right-of-way treatments embody the unifying and subarea themes described in this plan.
- Aesthetic treatments are visibly consistent for public and private lands when viewed from the corridor.

Principles:

C1. To improve aesthetic quality, bridges should contain a minimum of three (3) exterior materials, and include accent lighting in addition to standard lighting for safety.

C2. To improve aesthetic quality, no chain link fencing is allowed within direct view of the corridor or fifty-feet (50') outside the state owned right-of-way.

C3. To improve the aesthetic quality, fencing treatments should incorporate live, drought-tolerant vegetation, where direct transparency for safety is not required.

C4. To improve the aesthetic quality, landscape treatments, including flowering plants should be provided adjacent to identified gateways and landmark areas.

C5. To reduce a canyon effect in the corridor, vertical surfaces of walls should not exceed twenty feet (20') without a twelve inch (12") minimum horizontal break.

C6. To break down the scale of walls, patterns should be created that are a maximum of four feet (4') in any direction.

C7. To promote a pedestrian-oriented environment, pedestrian routes should be buffered from fast-moving traffic and expanses of parking.



A bridge integrating a gateway treatment and architectural pedestrian railing



Patterns can break down the scale of walls

Corridor Character Principles

D. Natural Spaces along the highway are primarily passive in form, and include open space, pedestrian pathways, waterways, wetlands and stormwater drainage areas.

Intents:

- Natural spaces complement and separate areas of nodal development and enhance the natural surroundings.
- Natural spaces represent interconnected systems and are organized to facilitate system-wide drainage.
- View corridors provide expansive views out of the corridor and identify landmarks when appropriate.



Principles:

- D1.** To facilitate pedestrian connectivity, natural spaces should create linear systems, particularly east-west along the corridor.
- D2.** To maintain a sustainable landscape, only native plantings should be provided.
- D3.** To promote expansive views, low plantings should be provided in open spaces designated to frame long views.
- D4.** To improve water quality, native landscape materials should be utilized to provide primary filtration of stormwater prior to entering sewers.



Low plantings preserve expansive views



Native plantings maintain a sustainable landscape

E. Access locations include the locations of curb cuts and intersections on frontage roads, arterial streets and intersections within the corridor.

Intents:

- Frontage roads facilitate local circulation parallel to the highway and provide access to the local street system.
- Local streets provide the majority of access to private property along the corridor.



Principles:

- E1.** To promote access, street patterns should form an interconnected grid that simplifies access for all transportation modes.
- E2.** To improve multi-modal circulation, bridges should include pedestrian paths and bicycle lanes.
- E3.** To increase capacity of the frontage road system, curb cuts should be minimized.



Retaining walls with unique character and treatments.

Corridor Character Principles

F. Nighttime treatments include the organization and design of safety lighting and accent lighting on spatial elements, including landmarks, key building and landscape treatments, bridge and wall treatments and open spaces.

Intents:

- Corridor treatments should be designed for daytime and nighttime users.
- Effect lighting attempts to replicate the daytime experience for nighttime users.
- Lighting for safety is integrated with effect lighting and provides a consistent design theme.



Principles:

F1. To improve the aesthetic quality, accent lighting should be included in right-of-way treatments.

F2. To express the design theme in each subarea, safety lighting should be incorporated with effect lighting.

F3. To promote walking during nighttime, pedestrian scaled lighting should be included on all walkways with one-quarter mile of defined nodes.



Pedestrian scale lighting promotes walking

G. Street design includes both traffic and pedestrian zones.

Intents:

- The traffic zone encompasses vehicular and bicycle movements, and can also include medians, crosswalks and on-street parking.
- The pedestrian zone includes all elements from the back of curb.



Principles: Traffic Zone

G1. Vehicular lane width will vary based on the street type and traffic volume. On highways such as IH-35E, 12-foot lanes may be most appropriate. Arterials with heavier volumes of traffic may require 11 to 12 foot lanes. On local streets, 10 to 11 foot lanes are encouraged to help reduce speed and narrow the overall traffic zone width.

G2. Bicycles may be accommodated either through a wider outside lane or through a striped bicycle lane. Bicycle lane widths should be between four and six feet. When placed adjacent to on-street parking, an additional 1 to 2 feet of bicycle lane should be provided.

G3. Medians should provide refuge areas for pedestrians and include special planting, paving treatments, or public art installations at each end to create a visual amenity.

G4. Crosswalks should include inset materials or textures, including brick pavers or stamped concrete which provides visual amenity and functional traffic calming for intersections.

G5. On-street parking is typically 8 to 10 feet wide allowing for parallel parking adjacent to the vehicular lane. On-street parking is typically placed on corridors with lower speed limits and streets with active pedestrian and development to help calm traffic and serve surrounding land uses.



Traffic zones need to balance vehicular and bicycle modes

Corridor Character Principles



Principles: Pedestrian Zone

G6. Sidewalks on frontage roads and arterial streets are encouraged to be detached from the curb and setback from the street a minimum of eight feet (8'). Sidewalk widths should be a minimum of six feet (6') and can exceed twelve feet (12') depending on planned pedestrian activity. Sidewalks on local streets should be a minimum of six feet (6') and either be attached or detached depending on other amenities provided.

G7. Curb cuts should be subject to conditions specified by an access management strategy for arterial roads. Curb cuts are only encouraged at combined business entries and for parcels that cannot be accessed from a local road. Curb cuts on all streets should be designed to be as narrow as possible while providing for safe traffic movements and access.

G8. Street trees are encouraged within the amenity zone. The trunk should be setback from the street a minimum of three feet (3') to provide a clear area from the street and to allow the canopy to mature.

G9. Planter boxes can be located in the amenity area and provide a buffer along busy streets and create a more intimate pedestrian space. Planters can be seasonal or permanent.

G10. Lighting along sidewalks should be provided to indicate safe pedestrian pathways. Pedestrian scale lighting can be incorporated with traffic lighting poles or provided as independent elements.

G11. Benches/seating on arterials should be provided at bus stops and near intersections in green areas to provide an amenity for people waiting to cross the street or board transit. Additionally, seating is encouraged in green areas with access to public open space, or large parking areas.

G12. Bus stops should provide overhead shelter from sun, snow and rain and provide side shelter from wind and rain. Side panels should provide a high degree of transparency and be clear of visual obstructions.

G13. Bicycle racks along streets should be provided adjacent to active streets containing neighborhood serving businesses. When provided, racks should be located in the amenity zone.



Benches and sidewalk treatments contribute to street character

Appendix B
Design Guidelines



Design Guidelines

These design guidelines are intended to enable the City to work together with the development and business communities in achieving the vision for IH-35E corridor. That vision includes four framework themes:

- Revitalizing the corridor will require enhancing the established character through multiple public and private realm improvements.
- Establishing memorable destinations will require creating authentic and diverse public places, while expanding the range of attractions and economic development opportunities that the corridor offers.
- Integrating the neighborhoods will require a mix of infill housing and services for local neighbors.
- Achieving a more accessible corridor will require improving the transportation system to minimize barriers and provide regional transportation alternatives.

The design guidelines are divided into two types: guidelines and standards. The Guidelines contained in this document are general statements describing ideal development along the corridor. The standards are basic requirements for any development or redevelopment along the IH-35E corridor. The use of these two types of statements is intended to give flexibility to the developer and/or applicant to respond and contribute to the corridor vision in advance of a submittal, to give the City of Lewisville a basis on which to make judgments so that its determinations are not arbitrary, and to give certainty to the City of Lewisville and its citizens that the corridor vision is met and that the quality described is maintained. The images in this section reflect examples from across the country, which exemplify the written standards.

As time passes and the city and its partners in the public and private sector advance in achieving the corridor vision, conditions along the corridor will change. The standards will be added to and amended over time. The guidelines and standards serve as a tool to ensure that the corridor vision and quality of corridor redevelopment remains consistently high.



Great places are defined in large part by great streets. Jane Jacobs said it well: “Streets and their sidewalks, the main public places of a city, are its most vital organs.”

Authority

These Guidelines shall be approved as part of the IH-35E Corridor Redevelopment Plan according to the provisions of the General Development Ordinance.

Applicability and Review

The review process follows the approved development process as stipulated in the City of Lewisville's adopted General Development Ordinance (GDO). The required standards of the corridor design guidelines and standards are in addition to the specific requirements of the GDO. These guidelines and standards supersede all other city design guidelines except as noted.

A design review committee (DRC) should be established by the City for review of projects along the IH-35E corridor. This committee should be responsible for the controls governing the design guidelines and standards. This DRC should have the responsibility to review all development within the boundaries of the corridor overlay area. Review of engineering requirements such as drainage; utilities; police, fire, and emergency service access is the responsibility of city staff. The DRC would have the responsibility of reviewing all design outside of the right of way, while city staff has the responsibility to review the street's engineering, traffic functions, utility and public safety requirements. The DRC should be composed of both public and private individuals. Representatives of the private development community bring value in providing their perspective.

Amendments

These Guidelines may only be amended by City Council with a recommendation from the Planning Commission.

Format

The format of the following design guidelines consists of development conditions defined by design principle statements and design standards. Development conditions are described for Site Plan, Architecture, Landscape, and Signage. Guidelines describe the design intent for each listed condition and should be incorporated into design treatments of each listed condition. Standards describe the specific treatments that, if incorporated, require no further DRC review. However, if standards cannot be achieved due to outstanding conditions, the DRC may evaluate specific condition proposed treatments against the stated Guidelines. If the DRC approves the proposed treatments no further DRC review is required. Appeals may be made to the Planning Commission.

Design Guidelines

A. SITE PLAN

A1 Building orientation

Guidelines

- A1.g1 The front facades and main entries of buildings should be oriented toward streets and plazas.
- A1.g2 Building orientation should provide views of adjoining publicly accessible streets and open spaces in order to provide passive viewing for safety.
- A1.g3 Pedestrian activity should be encouraged through the incorporation of active uses such as retail, commercial and/or institutional uses at the ground level.
- A1.g4 Buildings should define the street or public open space.
- A1.g5 Buildings should be located to promote sun and sky exposure to public streets and plazas.
- A1.g6 Buildings should be sited to create active outdoor spaces where possible, such as outdoor restaurant seating where appropriate.

Standards

- A1.s1 Buildings shall line a street at the Right Of Way or the build-to line to the greatest extent possible.
- A1.s2 Buildings shall use the full width of the lot for the primary structure and/or active outdoor space.

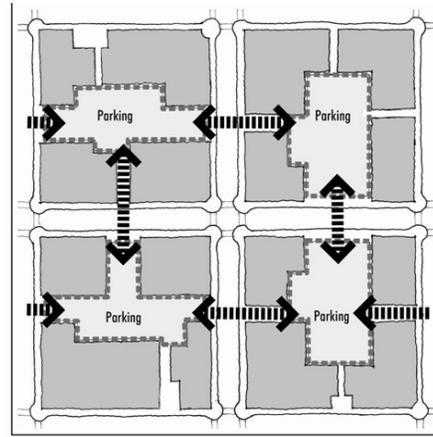


Facades and entries oriented toward street



Driveway across sidewalk identified by material change





A2 Access and driveways

Guidelines

- A2.g1 Access points, including alleys, and driveways should be located to promote the safe and efficient movement of vehicles, pedestrians and bicyclists.
- A2.g2 Uninterrupted pedestrian-ways should be maximized in order to improve walkability.
- A2.g3 The width of driveways and curb cuts should be minimized to reduce the overall impact of vehicular access across a sidewalk.
- A2.g4 Driveways and ramps to underground parking should be perpendicular or generally perpendicular to the street.
- A2.g5 Block frontages should have as few curb cuts as possible.
- A2.g6 Sharing of vehicle entries between two adjacent lots is strongly encouraged.

Standards

- A2.s1 Developments shall provide access for service vehicles via alleys or parking lots.

A3 Parking lot and structure location

Guidelines

- A3.g1 Buildings should be located to minimize the visual impact of parked vehicles within lots and structures.
- A3.g2 Parking lot location should minimize the impact of parked vehicles on the continuity of active commercial, mixed use, and/or residential frontages.



Identify clear access points for parking

Design Guidelines

- A3.g3 Parking lots and structures should be located to minimize the impact of vehicle noise and headlights from within parking lots and structures onto adjacent residential neighborhoods.
- A3.g4 Whenever possible, parking structures should be sited internally to the block so that parking structure street frontages are avoided. If internal siting is not feasible, then the parking structure should be oriented so that the shortest dimension fronts the street.
- A3.g5 If it is only feasible to orient the long dimension of a parking structure along a street, then the structure's street facade should exhibit the same high level of quality in its design, detailing and use of material as is provided in the adjoining commercial and/or mixed use buildings.
- A3.g6 Parking structures that are sited with exposed street frontage should orient the exposed frontage to commercial activities, rather than residential uses.

Standards

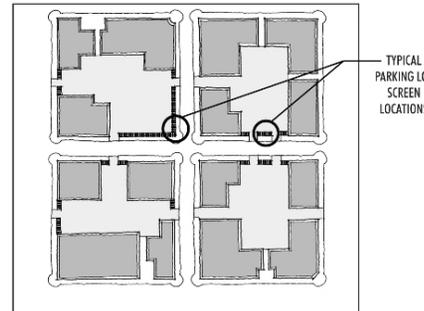
- A3.s1 Surface parking areas shall be located at the side or rear of buildings only.
- A3.s2 Parking structures with exposed street frontage shall not be oriented toward residential uses.



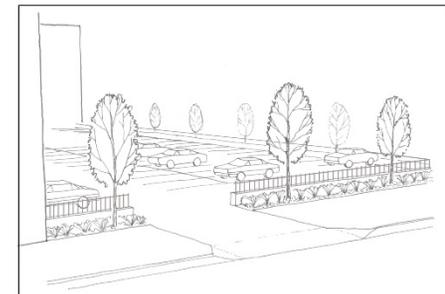
Active uses at street level of parking structure and high quality facade

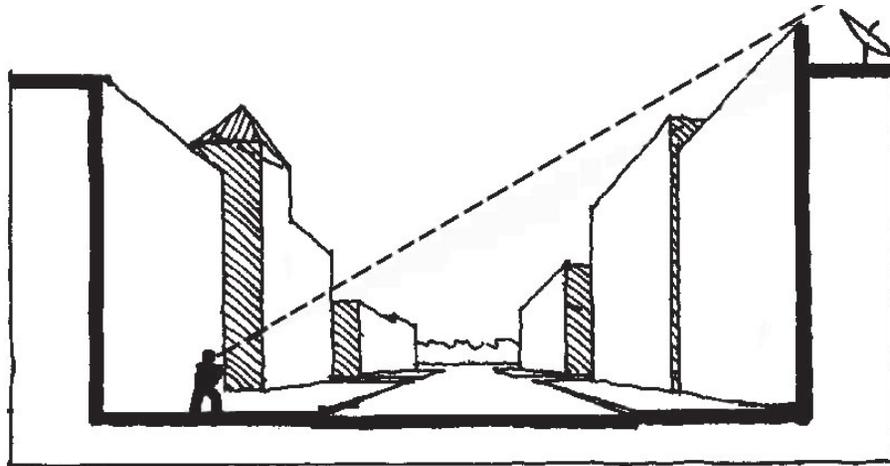


Parking structure with retail on ground floor, quality materials, and detail



Parking lot screen





Utility screening

A4 Utility location and screening

Guidelines

- A4.g1 Service areas and utility pedestals should be located to minimize the visual impact of service areas, refuse storage and mechanical/electrical equipment on streets, public open spaces and adjoining development.
- A4.g2 Utility appurtenances should be located behind the sidewalk and out of the sidewalk amenity zone wherever possible. Where it must be in the tree lawn or amenity zone, such equipment should be centered on the tree line and aligned with but no closer than 42 inches from the face of curb. This includes switch boxes, telephone pedestals, transformers, meters, irrigation, and similar equipment.
- A4.g3 The use of alleys is encouraged to locate all mechanical, electrical, and utility equipment to the extent possible.

Standards

- A4.s1 Service areas and refuse storage areas shall not front onto streets and public open spaces. Such areas shall be located to the rear or side of buildings, and screened from view from the street and/or public open space.
- A4.s2 Refuse storage and pick-up areas shall be combined with other service and loading areas.

Design Guidelines

A5 Pedestrian access

Guidelines

- A5.g1 Pedestrian entries to buildings should promote security on a street or public open space through frequent points of access and sources of activity.
- A5.g2 In general, ground floor uses with exterior exposure should each have an individual public entry directly located on a public sidewalk along a street, or on a sidewalk or plaza leading directly to a street.

Standards

- A5.s1 Primary building entrances shall be oriented toward streets, parks or pedestrian plazas.
- A5.s2 Each block face shall have multiple building entries. A building occupying an entire city block shall include more than one building entrance along each block face.
- A5.s3 All secondary building entries shall be well lit and directly connected to the street.



Primary building entrances oriented toward streets

B Architecture

B1 Building Character

Guidelines

- B1.g1 Building character should be creative and within a visually comfortable and familiar environment.
- B1.g2 Buildings should be designed to provide human scale, interest, and variety while maintaining an overall sense of relationship with adjoining or nearby buildings.
- B1.g3 Art integrated into building facades or forms, and/or specially designed architectural ornament is encouraged.

Standards

- B1.s1 All buildings shall be designed specifically for the context and character of the corridor. 'Iconic' corporate standard building design is encouraged at identified gateway and landmark locations.
- B1.s2 The majority of the building(s) of a development shall possess an architectural character that respects traditional design principles, such as:
 - Variation in the building form such as recessed or projecting bays;
 - Expression of architectural or structural modules and detail;
 - Diversity of window size, shape or patterns that relate to interior functions;
 - Emphasis of building entries through projecting or recessed forms, detail, color or materials;



Human scaled development with interest and variety



Variations of material, color, and texture



Design Guidelines

- Variations of material, modules, expressed joints and details, surface relief, color, and texture to scale;
- Tighter, more frequent rhythm of column/bay spacing, subdividing the building façade into smaller, more human scaled elements.

B2 Building Form

Guidelines

- B2.g1 New development should create occasional special building forms that terminate views, create a unique skyline, and aid in way-finding.
- B2.g2 Building form should emphasize important components of a building, such as an entry, or a special internal space.
- B2.g3 Lower building heights or upper level stepbacks are encouraged on the south or east side of the street or public open space in order to provide more sun penetration to the ground level.
- B2.g4 Taller buildings adjacent to lower buildings shall establish scale relationships with lower, neighboring buildings through methods such as: compatible horizontal alignment of architectural features and fenestration, and height and form transitions from one building to another.

Standard

- B2.s1 Building form shall employ a uniform level of quality on all sides of the building.



Example of upper level stepbacks



Emphasis on entry or special internal space



B3 Building Facade

Guidelines

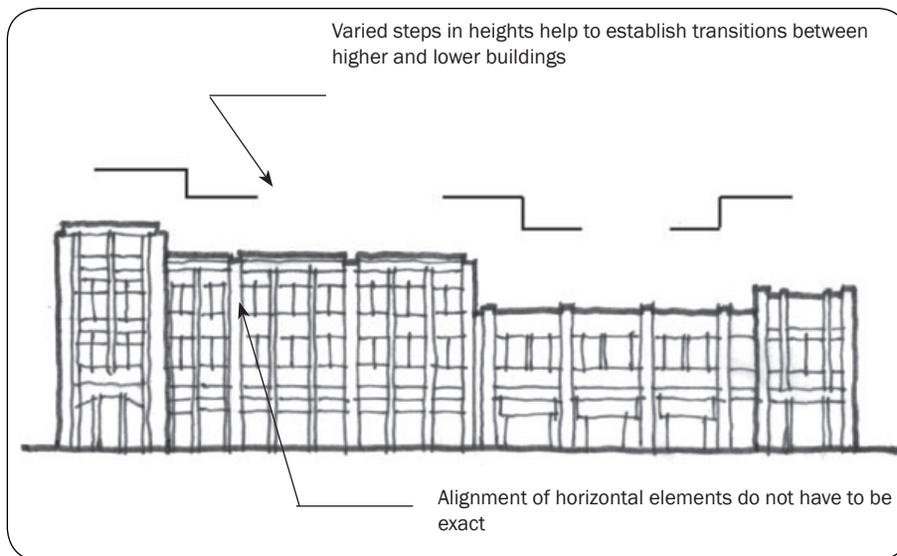
- B3.g1 Building facades should be designed to provide human scale and detail and to avoid large areas of undifferentiated or blank facades.
- B3.g2 Each building facade oriented to the street or public space should provide architectural variety and scale through the use of such elements as: expressions of building structure; patterns of window, door or other openings that provide surface variation through change of plane, change in color; change in texture; change in material module or pattern; art or ornament integral with the building.
- B3.g3 Primary building facades should include some elements that provide a change in plane that create interest through the interplay of light and shadow. Examples of such elements are:
- recessed windows, at least 3 inches;
 - recessed entries and doors;
 - projecting sills;
 - recessed or projecting balconies;
 - projecting pilasters, columns, bays;
 - projecting cornices, roofs.
- B3.g4 Each 'base' should be composed of the first floor or first two floors of the building.
- B3.g5 Each 'base' in its entirety should be designed to give the appearance of greater height than any single floor of the middle.



Varied steps in height



Vertical divisions in facade



Scale relationships

Design Guidelines

- B3.g6 Each 'base' should have a greater level of transparency than the 'middle' or 'top'.
- B3.g7 The architectural treatment of the 'top' should be designed to create a sense of distinctly completing the dominant architectural theme of the 'middle' of the building. This architectural completion may be accomplished by such strategies as: change in the window rhythm, change in apparent floor height, setback, use of other materials, or a combination of these elements.
- B3.g8 Distinctive corner, entry treatments and other architectural features designed to interact with contextual features may be designed differently than the 'base', 'middle', and 'top'. This difference would allow the addition of vertical emphasis at significant architectural points along the building facade.
- B3.g9 The 'top' of buildings above four (4) stories may have a 'cap' set back above the lower stories, which is distinctive in shape and smaller than the previous floor.

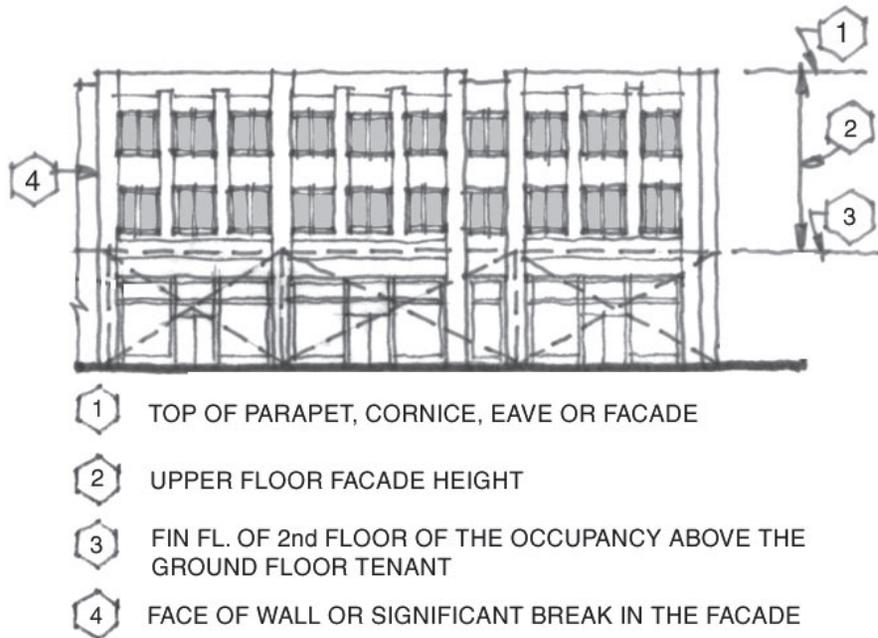
Standards

- B3.s1 The building facade shall generally have three vertical divisions: 'bases', 'middles', and 'tops'. In buildings of three stories or less in height, the 'top' may be comprised of an ornamental 'cap' or cornice rather than the articulation of an entire floor of habitable space.
- B3.s2 The design of 'roofscape' elements of tall buildings shall relate directly to the building walls.
- B3.s3 Building design shall create varied roof parapet and cornice lines in order to create interesting and human scaled skylines.



Vertical divisions in facade





Upper floor transparency



Building transparency



B4 Building transparency

Guidelines

- B4.g1 Where functionally appropriate, the ground floor, street-facing facade shall be made of transparent materials designed to allow pedestrians to view activities inside the buildings, retail goods for sale, or display lighted windows related to these activities.
- B4.g2 When transparency is not functionally appropriate, other means should be used to provide activity along the street-facing façade such as public art; architectural ornament or detailing; or material, texture, or color patterns.
- B4.g3 Buildings should incorporate a window or glazing-to-wall ratio that is sufficient to establish the visual solidity of the building form.
- B4.g4 Reflective glass should be used sparingly, if at all, to reduce glare, reduce the opacity or 'blankness' of the facade. Coated or tinted glass may be considered to reduce heat gain, particularly on west and south facades.
- B4.g5 Windows or glazing on upper levels should be sufficiently transparent to provide an awareness of internal activities when viewed from the street or public spaces.

Standards

- B4.s1 Glass without coatings or tints shall be used for all retail glazing. In no case shall highly reflective glass be used.

Design Guidelines

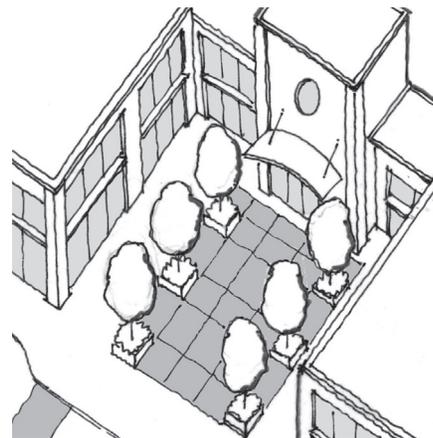
B5 Building Entries

Guidelines

- B5.g1 For mixed-use buildings with residential units, one or more separate building entrances from the sidewalk should be used to provide access to the residential units.
- B5.g2 Detailed and elaborate entries should be used as another way to create street level interest and architectural variety.
- B5.g3 Major building entries should be emphasized through such design devices as changes in plane, differentiation in material and/or color, greater level of detail, enhanced lighting, ornament, art, and/or building graphics.
- B5.g4 Primary building entries should be oversized, and generally break the storefront/ground floor façade pattern.

Standard

- B5.s1 Each multi-story building shall have one clearly identifiable 'front door' that addresses the street. In addition to this 'front door,' a building occupying an entire city block shall include at least one other building entrance along each block face.



Emphasis on building entry





High quality materials



B6 Building Materials

Guidelines

- B6.g1 New development should use materials and colors that possess a comfortable and familiar character, convey a sense of quality and attention to detail, and are compatible with materials of adjacent buildings.
- B6.g2 New development should use lasting materials that weather well, need little maintenance, and resist vandalism.
- B6.g3 Materials and/or detailing at retail frontages should distinguish between the structural parts of a building (columns, walls and beams), and the infill parts of a building (wall panels, frames, windows and doors).
- B6.g4 Infill materials should have a non-structural appearance.

Standards

- B6.s1 A significant portion of the facade facing a street or public open space (not including windows, doors and their framing systems), shall be composed of highly durable materials such as: brick, stone, cast stone, specially treated concrete masonry units, terra-cotta, and/or glass. All building materials shall be integrally tinted.
- B6.s2 Building materials shall maintain a uniform level of quality on all sides of the building.

Design Guidelines

B7 Parking Structures

Guidelines

- B7.g1 The exterior of parking structures should be wrapped with mixed-use space in order to minimize the visual impact of parking on the pedestrian experience, and the street environment and to increase pedestrian activity and interest along the street by locating active uses at the street level of parking garages
- B7.g2 Garage facades visible from public streets and open spaces should be compatible in character and quality with adjoining buildings.
- B7.g3 Parking structures should create visually interesting facades that provide human scale and detail while avoiding large areas of undifferentiated or blank facades.
- B7.g4 Openings should be vertically and horizontally aligned.

Standards

- B7.s1 Street oriented facades shall conceal or effectively reduce the impact of parked cars and light sources from the exterior view for the full height of the structure.
- B7.s2 Multi-story parking structures (3 levels or more) with facades facing public streets shall provide commercial, live-work, residential and/or institutional space for not less than 50% of the garage's ground level street facing frontage, or the design and structure of the ground floor street frontage should be able to accommodate in the future one of the above listed uses.
- B7.s3 Sloping ramps shall not be visible within the street facade of any parking structure.



Retail wrap and compatible facade on upper stories of parking structure



Street facade of parking structure that screens parked cars



B8 Building Lighting

Guidelines

- B8.g1 Building lighting should accentuate important architectural components of the building, such as entries, towers or roof elements, or repetitive columns or bays, and include decorative lighting.
- B8.g2 Building lighting should provide indirect or direct lighting for adjoining sidewalks and open spaces.
- B8.g3 Primary building entries should be externally lit so as to promote a more secure environment at the door, emphasize the primary point of entry into the building, and provide sufficient lighting for efficient access into the building.
- B8.g4 Steps and/or ramps at or leading to a primary building entry should be illuminated sufficiently for safe access.

Standard

- B8.s1 Entry lighting shall complement the building's architecture. Standard security lighting such as wallpacks shall not be allowed.



Lighting at building entries



Design Guidelines

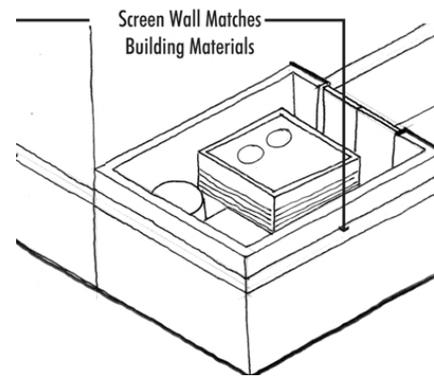
B9 Rooftop design

Guidelines

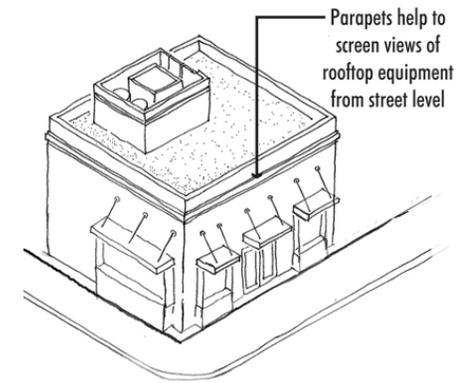
- B9.g1 Rooftop design should maintain the integrity of architecturally designed building tops and help create interesting and varied skylines.
- B9.g2 In mixed use development, if residential uses are located near mechanical equipment, care should be taken to mitigate the impacts of noise and odors.
- B9.g3 Antennae that extend over five feet above the roof line are encouraged to have screening techniques applied such as color and material to minimize visibility.
- B9.g4 Streetscape within the corridor area should not be cluttered by utility elements.
- B9.g5 Utility boxes should be located so that they do not obstruct pedestrian traffic or block sight lines at intersections.

Standards

- B9.s1 All roof mounted mechanical and electrical equipment, communication antennae or dishes shall be enclosed, screened, organized, designed and/or located as part of the architectural expression and shall not be visible from the public right of way. Any equipment shall be covered or screened to its full height.
- B9.s2 Switch boxes, transformers, electrical and gas meters, and other above ground utility elements shall be screened or located out of view from the street.



Screening of rooftop mechanical equipment



C. LANDSCAPE

C1 Perimeter Landscaping

Guidelines

- C1.g1 Perimeter landscaping design should create street and plaza spaces that join buildings, uses, pedestrian areas, and streets into a unified urban place.
- C1.g2 Perimeter landscaping should reinforce the pedestrian environment established in the adjoining street right of way.
- C1.g3 Perimeter landscaping should be designed to provide seamless transitions between buildings, uses, and open spaces that promote the mixing of commercial, residential, and institutional uses.
- C1.g4 Where a landscape perimeter area occurs between a building frontage and a street right of way, it should be designed to extend the pedestrian amenities of the street, such as increased walkway widths, areas for outdoor café/restaurant seating, increased sidewalk widths to allow window shopping out of the stream of pedestrian traffic, and space for the temporary display of a retailer's goods.
- C1.g5 Where space permits, planting in containers, raised planters, or cutouts in the paving is encouraged.

Standard

- C1.s1 Where a side setback landscape perimeter area occurs, it shall be designed to contribute to a pedestrian amenity zone such as a passageway, or contribute to a paved driveway or alley.



Perimeter landscaping design



Design Guidelines

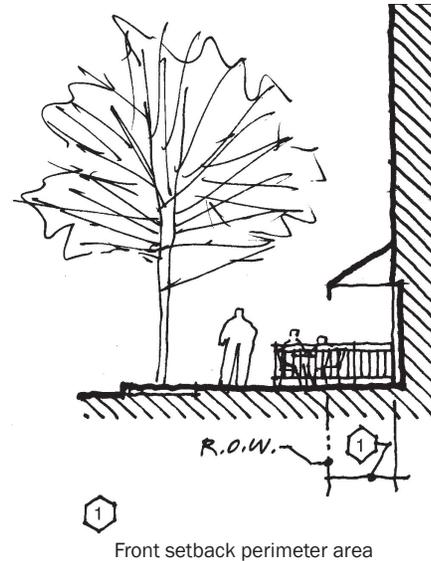
C2 Internal courtyards, plazas and open spaces

Guidelines

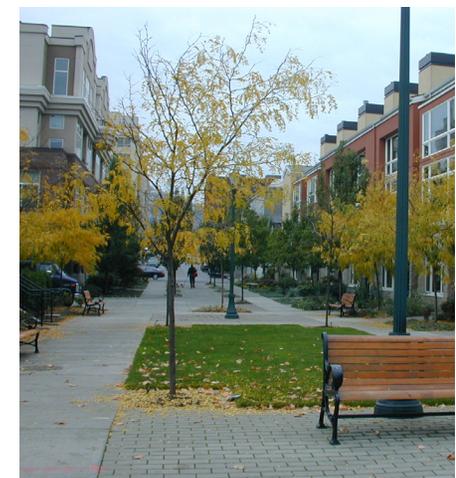
- C2.g1 Internal courtyards, plazas, or open spaces should be designed to create useable open spaces, suitable for passive recreational activities such as informal play, reading, and sitting in the sun or shade.
- C2.g2 All open spaces accessible to the general public should be open a minimum of 12 hours per day.
- C2.g3 Private open space may be fenced with wrought iron, masonry or comparable decorative fencing or otherwise controlled for security.

Standard

- C2.s1 All public and private open space not used for recreation shall be attractively landscaped with plant material and hard surfaces.



Internal courtyards



C3 Hardscape Design

Guidelines

- C3.g1 Hardscape design should provide a quality of paving materials and patterns consistent with the quality of the surrounding architecture and open spaces and provide safe paving conditions for all persons.
- C3.g2 Hardscape design should create interest and variation within paved surfaces that includes but is not limited to public art, coloring, or materials.
- C3.g3 Special paving should be carefully chosen for structural capability and durability in the local climate. Uncolored concrete, colored concrete, brick, hydraulically pressed concrete unit pavers or stone is recommended.
- C3.g4 Special paving patterns and materials should be used to emphasize important building entries, provide interest and variation, and differentiate between sidewalks, plazas, medians, and crosswalks.

Standards

- C3.s1 Sidewalks shall be separated or buffered from vehicle travel lanes by street/pedestrian lights, and/or street trees in grates or in a tree lawn.
- C3.s2 In transition areas, sidewalks shall be separated from the street by trees in tree lawns.



Variety in sidewalk paving materials



Design Guidelines

C4 Landscape: Trees and Plant Materials

Guidelines

- C4.g1 Landscaping should create a strong identity for each street and use quality plant materials that are located, sized, and provided in quantities sufficient to emphasize important streets.
- C4.g2 Landscaping should use plant materials that tolerate an urban condition.
- C4.g3 Trees should align parallel and perpendicularly across the street with each other whenever possible.
- C4.g4 Ornamental trees should not be used in a street right-of-way.
- C4.g5 Tree grates or planting cut-outs should be used in paved areas to prevent excessive soil compaction.
- C4.g6 Large tree pits that allow for a broader canopy are preferred over typical street trees.
- C4.g7 All tree lawns and street trees in cut-outs, tree pits, and grates should be irrigated with an automatic irrigation system. Drought tolerant turf or low, continuous ground covers should be used as the primary ground cover for continuous tree lawns.
- C4.g8 To the maximum extent feasible, topsoil that is removed during construction activity should be conserved for later use on areas requiring re-vegetation and landscaping.



Tree lawns in right-of-way



Design Guidelines



Pedestrian lighting that provides an identity



Alignment of pedestrian lighting

Standards

- C4.s1 No artificial trees, shrubs, turf, or plants shall be used to fulfill the minimum requirements for landscaping.
- C4.s2 Tree lawns shall be a minimum of 6 feet in width, measured from the back of curb to the edge of the sidewalk.
- C4.s3 Street trees shall be centered within the width of the tree lawn.
- C4.s4 Street trees in tree grates shall be at least 2 feet 6 inches from the face of the curb. Tree grates shall be at least 24 sq. ft. with openings no more than 1/4 inch to 3/8 inch in width and should be designed to allow for tree trunk growth.

C5 Street and Pedestrian Lighting

Guidelines

- C5.g1 Lighting should provide a safe and secure environment for motorists, bicyclists, and pedestrians.
- C5.g2 Lighting should create an identity for the development and/or special streets.
- C5.g3 Lighting should enhance the quality of streets in the commercial core through the design of the light poles, bases, fixtures, and attachments.
- C5.g4 Street and/or pedestrian light poles should be aligned with and centered between street trees.

Design Guidelines

- C5.g5 Where the light source is directly visible, the luminaries should be designed to incorporate elements to reduce glare, such as translucent, internal refracting surfaces to direct light down and away from adjoining private property; lower height poles; lower wattage or pole location.

Standards

none in this section

C6 Street Furniture

Guidelines

- C6.g1 Seating should be durable, comfortable, attractive, securely anchored, and easy to maintain. Seating surfaces should be 16 to 18 inches high with a minimum depth of 16 inches for seats without backs and 14 inches for seats with backs.
- C6.g2 Where bus stops occur within tree lawns, a minimum of one 6-foot long bench should be placed on a concrete pad. Where a bus stop occurs on a wide attached sidewalk, a 6 foot long bench should be provided within the sidewalk's amenity zone.
- C6.g3 Trash receptacles should be conveniently located near benches and other activity nodes.
- C6.g4 Trash receptacles should relate in appearance and color to other street furniture. They should be firmly attached to paving to avoid vandalism. Covered tops and sealed bottoms should be included to keep the contents dry and out of sight at all times.



Consolidated newsracks

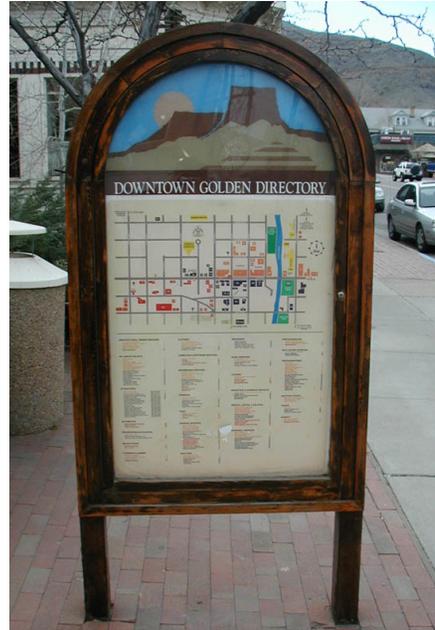


Durable and comfortable seating

Design Guidelines



Wayfinding signs



- C6.g5 Bicycle racks should be placed near entrances or gathering places, but out of pedestrian and bicycle traffic areas where they may create tripping or other safety hazards. If possible, locate racks where parked bicycles are visible from the inside of adjacent buildings.
- C6.g6 Newspaper racks and trash receptacles should be located at areas where high pedestrian activity is anticipated.
- C6.g7 Newspaper boxes should be clustered together and screened by specially designed railings. They should be located adjacent to pedestrian activity, but not so as to obstruct drivers' views at intersections, or car overhang/door swings at the curb.

Standards

none in this section

C7 Wayfinding Elements

Guidelines

- C7.g1 Wayfinding should compliment and enrich the pedestrian experience and create interesting streets and spaces.
- C7.g2 Wayfinding information should be conveyed clearly and efficiently with high quality sign and graphic design.
- C7.g3 Information should be provided for events on-site as well as within the City.

Design Guidelines

- C7.g4 To provide art, whimsy and contrast to the civic structure of the street furnishings, wayfinding elements should relate to local culture and flavor.
- C7.g5 Information kiosks and wayfinding elements should be located near pedestrian origin points such as parking structure stairs and elevators, public plazas and near entrances to public buildings.

Standards

none in this section

C8 Gateway Elements and Public Art

Guidelines

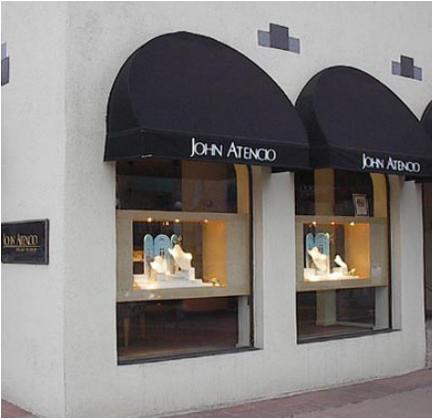
- C8.g1 Public art should engage the community, and express community identity.
- C8.g3 Art should create experiences for the senses and opportunities for surprise, wonder, interest, contemplation, reflection, humor, interaction and play.
- C8.g4 Art should provide shade structures at appropriate locations, particularly on the north side of the street.
- C8.g5 Commissioned works should exhibit superior craftsmanship and design, and be fabricated of durable, low maintenance materials using proven technologies. A range of signature pieces should include integrated urban design elements, architectural detailing and interactive features.
- C8.g6 Art should be sited to create areas of emphasis within the urban fabric while supporting the social function of each space.



Public Art



Design Guidelines



C8.g7 Selected artworks should include interactive elements allowing residents and visitors to walk through, play, sit on, and otherwise physically interact with the finished work.

C8.g8 Artwork, where appropriate, should be integrated into infrastructure and site furnishings (i.e. hardscape/landscape elements, building facades, tree grates, wayfinding devices, seating, etc.).

Standards

C8.s1 All plaza areas shall include public art.

C8.s2 Artwork shall be designed and sited to correlate with surrounding activity patterns.

D. Signage

D1 General Criteria

Guidelines

D1.g1 Signs should be located, sized, and designed for single or multiple uses so as to eliminate conflicts, predict the impact and effects of the signs on adjoining properties, avoid clutter and achieve the desired character of their application.

D1.g2 In an effort to limit the variety of sign types used on a single building along the corridor, the following combinations should be considered:

- One (1) wall sign per use; window signs limited to 10 percent of any window area; one (1) monument sign per building frontage, but awning signs, pole signs, or projecting signs are discouraged in this combination.

Design Guidelines

- Window signs limited to 20 percent of the window area, awning signs, and one (1) projecting sign per use, but wall signs, pole signs, or monument signs are discouraged in this combination.
- One (1) wall sign per use, one (1) projecting sign per use if located or designed so as not to visually conflict, window signs limited to 10 percent of any window area, but awning signs, pole signs, or monument signs are discouraged in this combination.

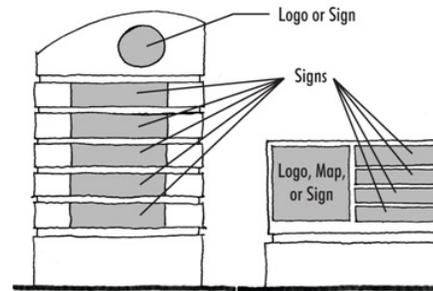
Standards

- D1.s1 Rehabilitated buildings shall provide a sign plan showing locations, sizes, heights, and probable design and illumination of all sign types to be used on the building or its site.

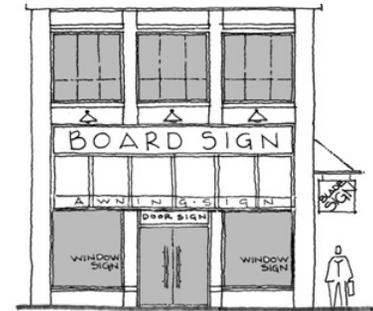
D2 General Number and Location of Signs

Guidelines

- D2.g1 Signs should be limited in number commensurate with the needs of the uses in the building.
- D2.g2 Signs should respect the architectural character and design of the building in their number and location.
- D2.g3 Sign clutter, where the number and size of signs dominate the storefront or façade of the building, should be avoided.



Signage examples



Awning sign



Projecting sign

Design Guidelines



Wall sign



Window sign

Standards

- D2.s1 Wall, window, awning, and projecting signs shall not be allowed above the ground floor with the exception of the following with the discretion of the design review committee:
- Painted, face-lit wall signs;
 - Internally lit channel letter signs and/or logos;
 - Painted wall murals with a minor component for the identification of a business;
 - One unlit window sign per business;
 - The extension of a ground floor projecting sign;
 - The name of the building integrated into the material and/or design of the facade; In no case shall an internally lighted, cabinet type wall sign be allowed above the ground floor.
- D2.s2 Signs shall not be located within the residential portion of the facade of any mixed use building.
- D2.s3 A maximum combination of three sign types shall be used for any building frontage. Such sign types are: wall, projecting, ground, window, awning, marquee and arcade.

D3 General Size and Height

Guidelines

- D3.g1 The size of signs should be related to the location and speed of movement of the typical person viewing the sign.

Standards

none in this section

Design Guidelines

D4. General Design and Illumination

Guidelines

- D4.g1 Signs should respect the architectural character and design of the building.
- D4.g2 Signs should be expressive of the activity, product, or use for which they are displayed.
- D3.g3 Signs should be compatible with existing residential uses.

Standards

- D4.s1 Materials for signs shall compliment the color, material and overall character of the architecture.
- D4.s2 Signs shall be constructed of high quality, durable materials. All materials must be finished to withstand corrosion. All mechanical fasteners shall be of hot-dipped galvanized steel, stainless steel, aluminum, brass or bronze.
- D4.s3 All conduits, transformers, and other equipment shall be concealed, and shall have UL ratings.
- D4.s4 Exterior lighting of signs shall be oriented down onto the face of the sign, not up from below to minimize night sky light pollution.
- D4.s5 Sign illumination shall not create objectionable glare to pedestrians, motorists, and adjoining residents.
- D4.s6 A business's corporate logo or typical sign design may be allowed by the design review committee. However, the design review committee shall retain complete control over the design, dimensions, location, number and type of the sign.
- D4.s7 Hand painted signs shall not be allowed, unless painted by a sign contractor specializing in hand painted or hand crafted signs.



Signs compliment color, material and character of architecture

Design Guidelines



Acceptable projecting wall sign



Wall sign with mounted letters

D5 Wall Signs

Guidelines

- D5.g1 Wall signs should be integrated with the architecture of the building.
- D5.g2 In general, wall mounted sign cabinets should be discouraged.

Standards

- D5.s1 Wall signs shall be located within any sign areas clearly designed for signs on existing or proposed building facades.
- D5.s2 Lighted wall signs shall not be located at the top of a building's facade if the facade is higher than two stories and shall not directly face a residential neighborhood.
- D5.s3 Maximum wall sign size shall not be increased by an increase in sign height.
- D5.s4 No more than one wall sign shall be allowed per building.
- D5.s5 Wall signs shall not overlap, or generally conflict with important architectural features such as windows, cornices, belt courses, or other details.

Design Guidelines

- D5.s6 Wall signs located on the side wall of a building that faces a side property line, alley, or parking area (including a side property line along a street), shall not be lighted above the ground floor.
- D5.s7 Wall signs shall be composed of individually mounted letters, logos or icons without sign backing panels, or letters/logos mounted on a backing panel.
- D5.s8 Phone/Fax numbers on all signs, with the exception of window signs, shall not be allowed.
- D5.s9 Neon signs, except those located in a window, shall not be allowed.



Appropriately scaled lighting and signage

D6 Projecting Signs

Guidelines

- D6.g1 Projecting signs should not be closer than 50 feet apart, and no more than 3 for 300 feet of street frontage.

Standards

- D6.s1 Each use by right shall be limited to one projecting sign for each of that use's street frontage.
- D6.s2 Projecting signs shall not be located above the ground floor.
- D6.s3 All projecting sign structures on a building shall be located at the same height as the other sign structures.
- D6.s4 Projecting signs shall be located above or below non-signed awnings, but not in line with the awnings.



Desirable ground sign



Desirable window signs

- D6.s5 Projecting signs shall not be greater in size than 12 square feet per face or 24 square feet per sign.
- D6.s6 Projecting signs shall be externally lit. Internally lit sign cabinets are generally discouraged except where the sign face is composed of metal with back lit cut out letters or logos.

D7 Ground Signs

Guidelines

- D7.g1 Ground signs should be refined, creative and unique.
- D7.g2 'Designed' pole or post signs are encouraged when the vertical supports are integrated into the design of the sign.
- D7.g3 The design of a joint identification sign should be unified, uncluttered, easily readable, and of high quality. Ways to avoid a cluttered appearance are:
 - The sign text for most components is composed of the same type face and size.
 - The sign structure or frame is dominant enough or simple enough to visually organize varied components.
 - The sign has a clear hierarchy or importance in its components.

Standards

- D7.s1 Only one (1) monument or per street frontage sign shall be allowed per building. The monument sign may also be a joint identification sign.
- D7.s2 Ground signs shall have no more than one sign cabinet or backing panel.
- D7.s3 If lighted, monument signs should be externally lit with a shielded or directed light source.

D8 Window Signs

Guidelines

- D8.g1 Window signs should emphasize a window's transparency and sense of openness to the interior.

Design Guidelines

- D8.g1 Window signs should avoid clutter 1) within the text and graphic components of the window signs, and 2) in combination with the objects of view through the window.

Standards

- D8.s1 Window signs shall generally be located in the lower or upper 25 percent of the window area. Window signs may be located in the middle portion of the window, but should not substantially obscure the activities or displays beyond the window.
- D8.s2 Window signs should not be larger than 10 percent of each window or door area, except that window signs may be as large as 20 percent of each window area if no wall sign is provided.
- D8.s3 Storefront window signs shall be limited to either the tenant's name or logo. Operating hours may be applied onto the glass, but shall be kept small, preferably on the windows next to the front door.
- D8.s4 Window signs on glazing shall be either silk screened, back-painted, metal-leafed, or sand-blasted onto the glass. Vinyl letters are not allowed.



D9 Awning Signs

Guidelines

- D9.g1 Awning signs should be carefully controlled so as not to become substitutes for wall signs or projecting signs

Standards

- D9.s1 Each awning may have a sign printed on its valence.

Design Guidelines



Desirable awning signs



- D9.s2 Awning signs shall not be allowed above the ground floor. Awnings without signs may be allowed above the ground floor if they are compatible with the architecture.
- D9.s3 Awnings shall be consistent in color and visually balanced over the façade of the building.
- D9.s4 Standard residential type aluminum awnings shall not be used. Awnings shall be composed of non-combustible acrylic fabric.
- D9.s5 Back-lit translucent awnings with or without signs shall not be allowed. Shielded down lights within an awning that light only the paving under the awning may be acceptable.
- D9.s6 Entry canopies shall not be allowed if they extend more than 4 feet from the building face.
- D9.s7 Awning signs shall be located primarily on the awning valence that faces the street, not on a valence that is generally perpendicular to the street.
- D9.s8 If side panels are provided, such panels should not carry signs greater in area than 20 percent of the area of the awning sign panel.
- D9.s9 Text on awning valences shall not be greater than 8 inches high. A valence drop length shall be no greater than 12 inches.
- D9.s10 Awnings shall not extend vertically beyond a building's or storefront's individual bays.
- D9.s11 Awnings shall be composed of traditional forms, and compliment the window or bay within which it occurs. Straight, more steeply sloped awnings are preferred. Rounded 'barrel' awnings are discouraged. Rounded awnings designed to fit arched windows or bays are acceptable.

Appendix C
Illustrative Plans

Northern Sub-District Illustrative Plan

The Illustrative Plans are intended to guide elected officials, developers, and city staff on the character and form of redevelopment desired. They are not regulatory.



Main Street Sub-District Illustrative Plan



Northern Sub-District Illustrative Plan

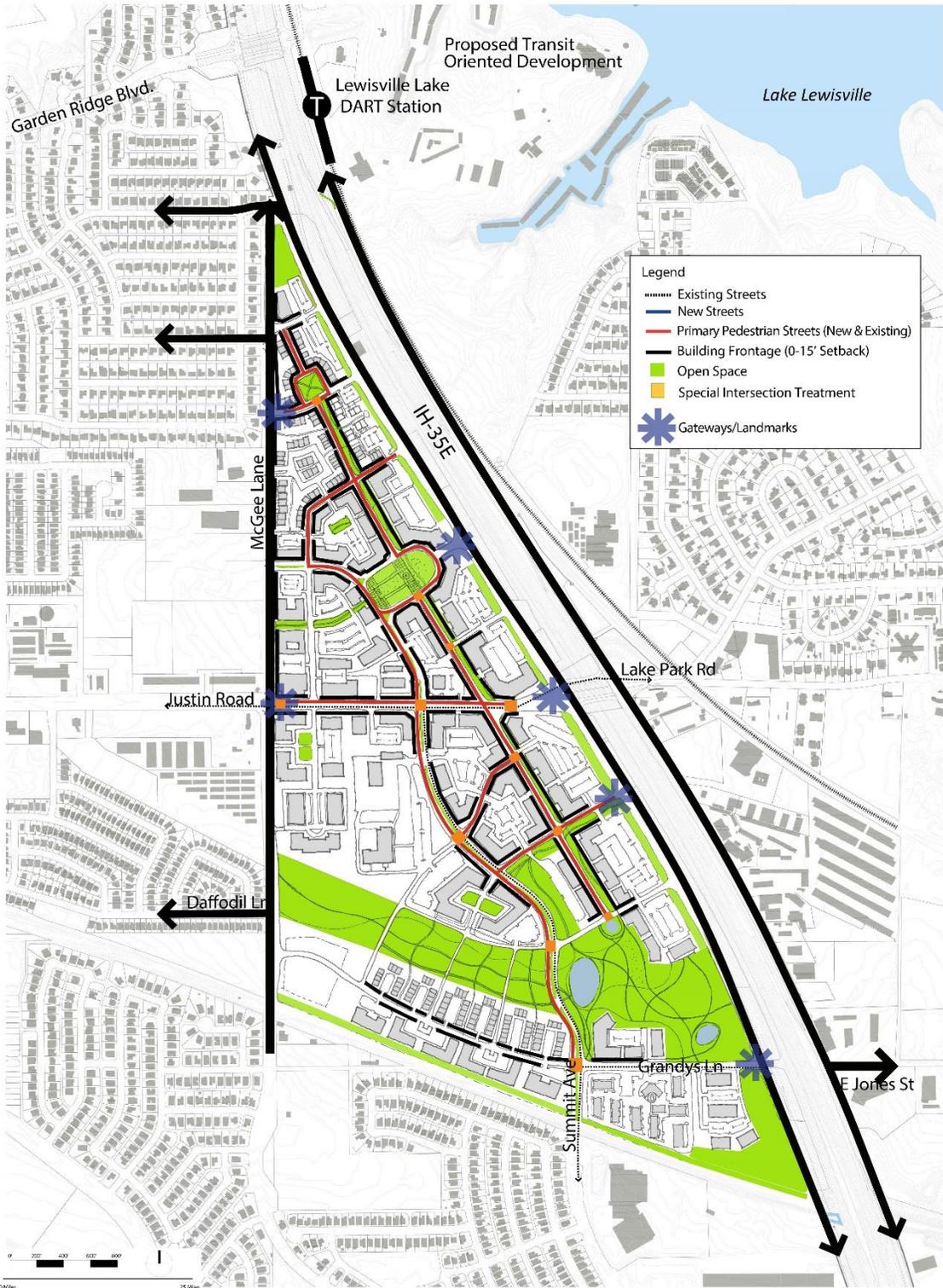


Southern Sub-District Illustrative Plan

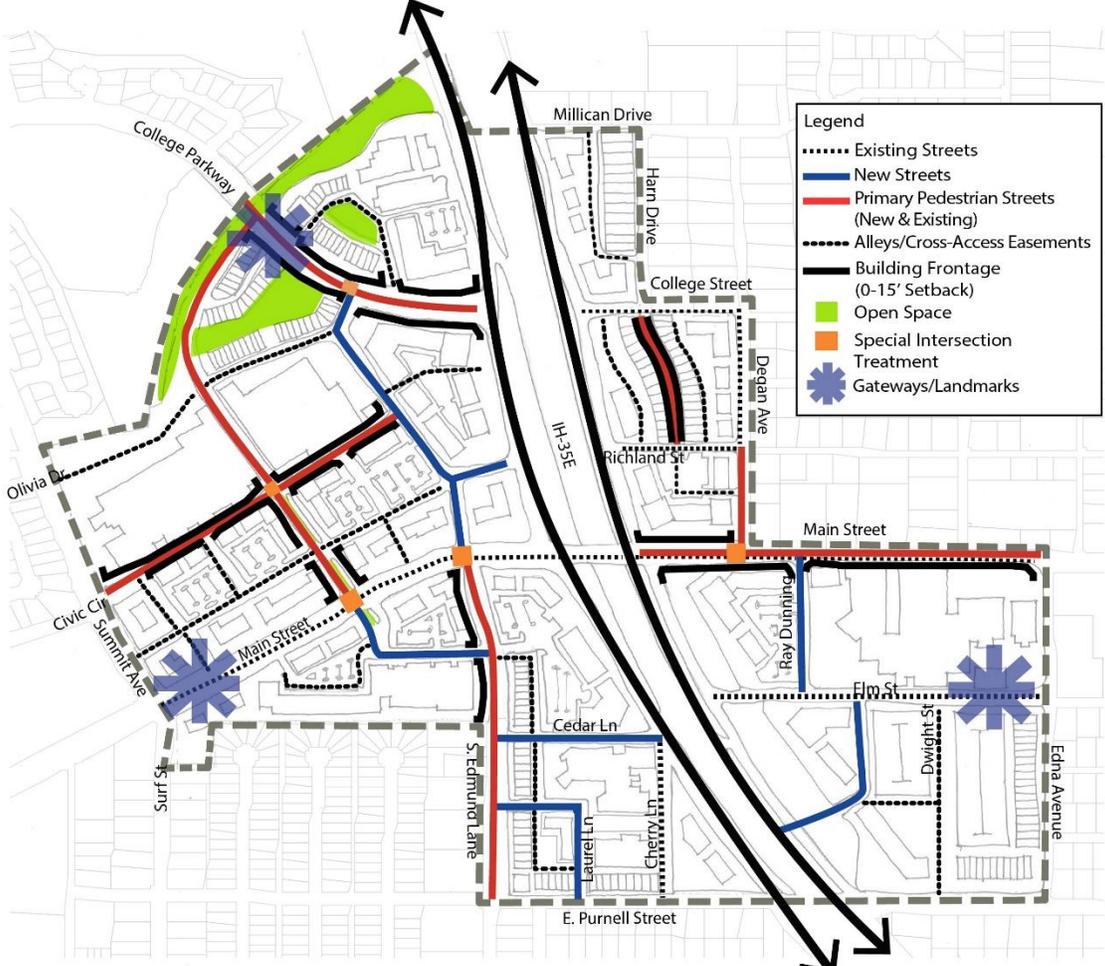


Appendix D
Framework Plans

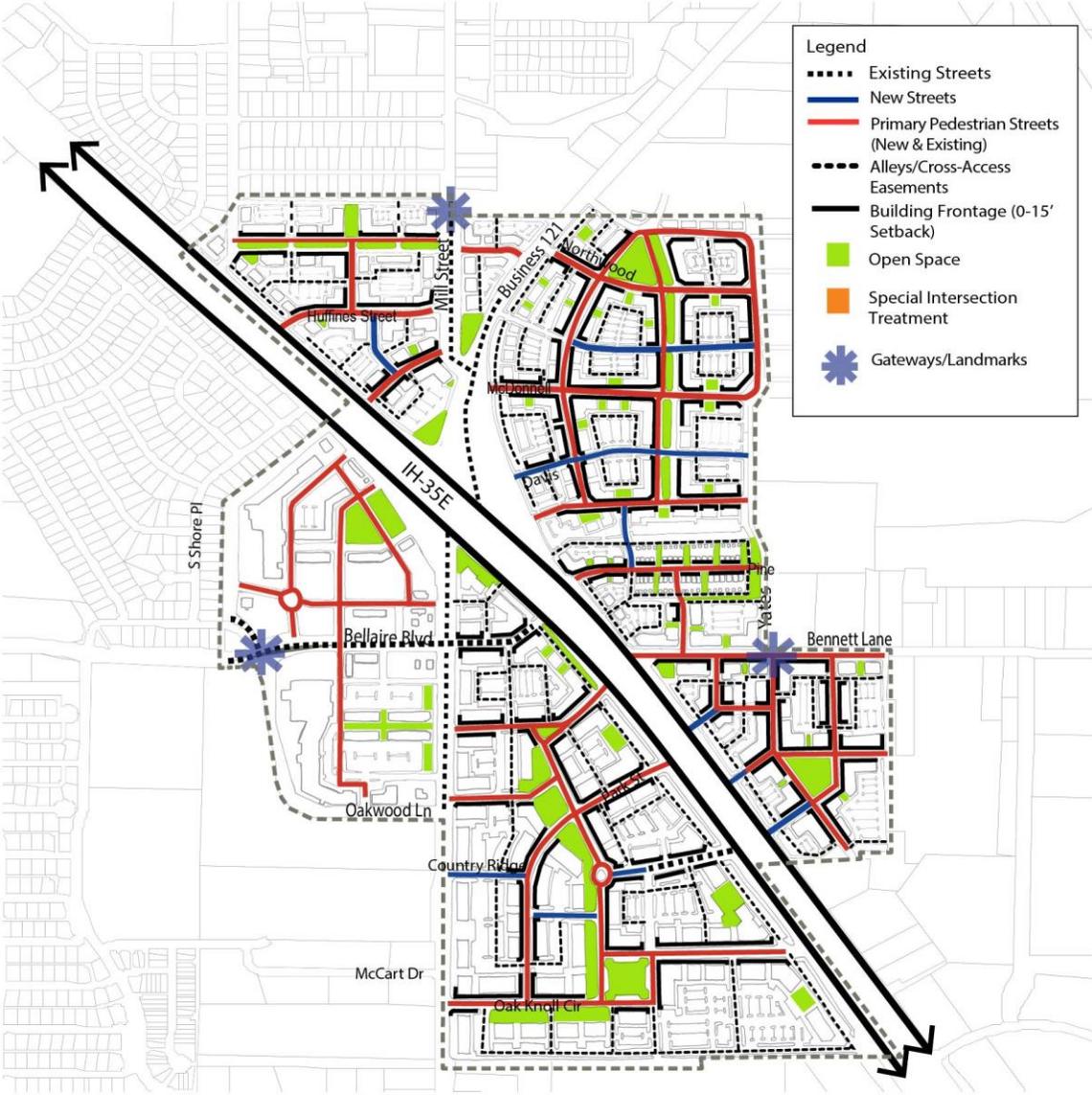
Northern Gateway Core Sub-District Framework Plan



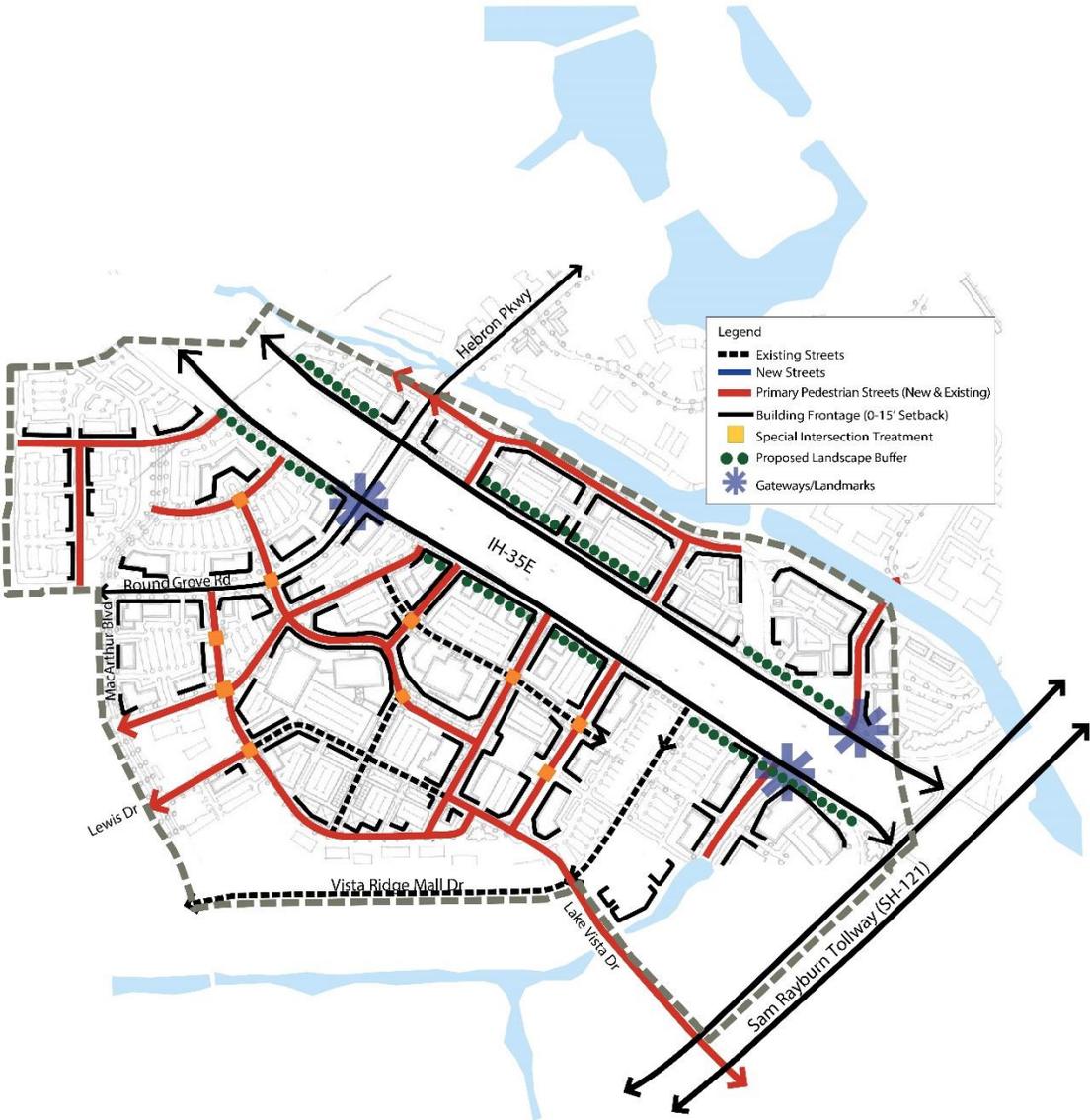
Main Street Core Sub-District Framework Plan



Central Core Sub-District Framework Plan

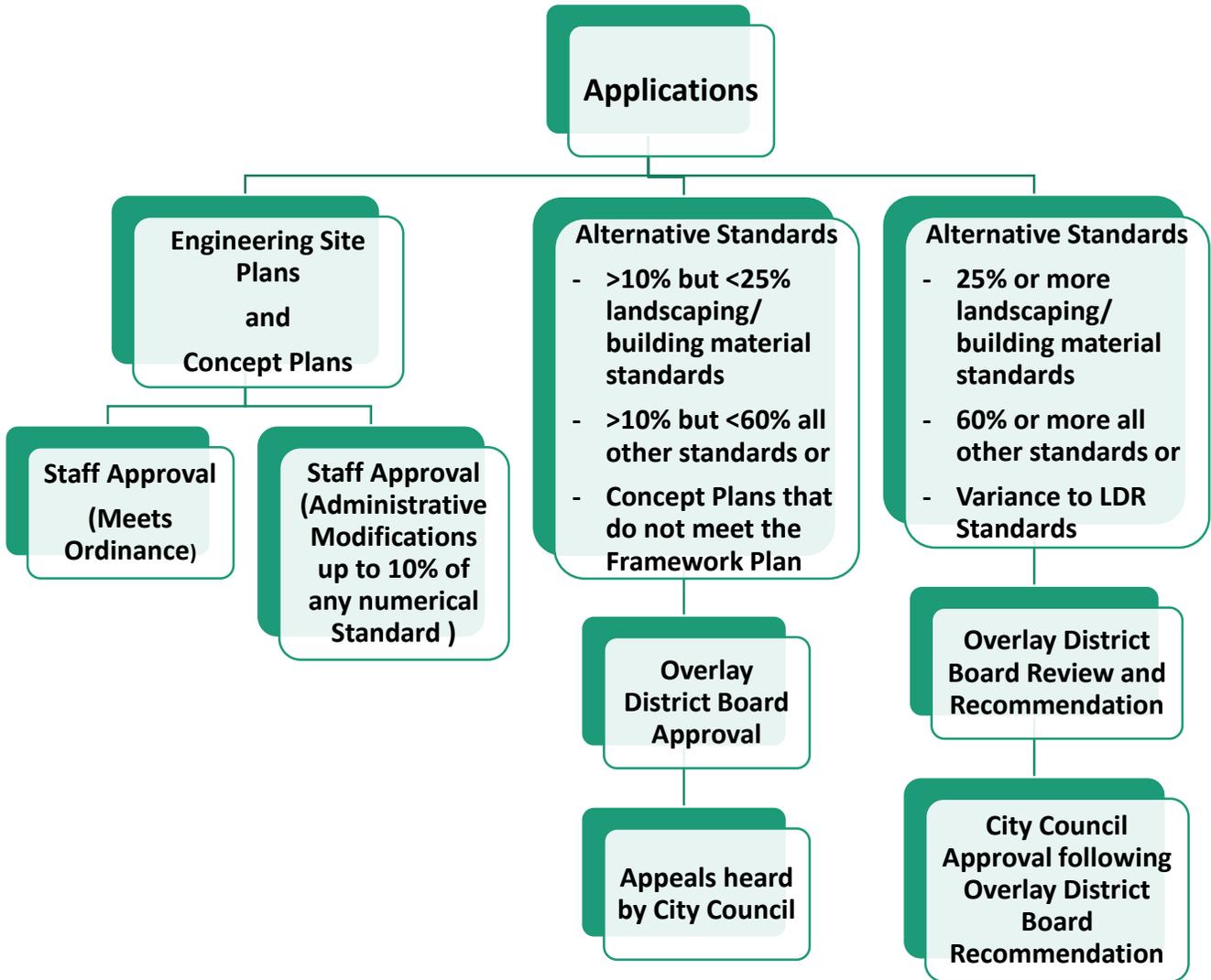


Southern Gateway Core Sub-District Framework Plan



**Appendix E Process
Flow Chart**

Appendix E Process Flow Chart



Appendix F Core Sub-district Street Design Standards

Street Types →	Arterial	Collector (Divided)	Collector (Undivided)	Local Street/ Private Vehicular Route		Alley	
				Commercial	Residential	Commercial	Residential
ROW* (min-max)	74'-106'	84'-118'	52'-93'	52'-86'	52'-58'	32'	18'-58'
Design Speed	45 MPH	35-45 MPH	25-35 MPH	25 MPH	25 MPH	<20 MPH	<20 MPH
Travel Zone							
Number of Lanes (min-max)	4-6	4	2-3	2	2	2	1 Yield
Lane Widths** (min-max)	11'	10'-11'	10'-11'	10'-11'	10'-11'	11'	Yield# (12'-15')
Median Required (Y/N; min-max)	Y (14'-16')	Y (12'-14')	N	N	N	N	N
On Street Parking Permitted (Y/N)	N	Y	Y	Y	Y	N	Y
On-Street Parking Required (Y/N)	N	Y	Y	Y	Y	N	N
Allowable Parking design and Min. Width	Parallel (8' min.)	-	Y	Y	Y	-	-Y
	Angled (60 degree, 18'	-	Y	Y	Y	N	-Y
	Head-in (20' min.)	-	N	N	Y ^e	N	-
On-Street Bike Lane *** Permitted (Y/N)	-N	Y	Y	Y	N-	-N	-N
Streetscape Zone							
Total Streetscape Zone							
Primary Pedestrian Street	12' (min.)	12' (min.)	12' (min.)	12' (min.)	10' (min.)	NA	NA
All other Streets	8' (min.)	8' (min.)	8' (min.)	8' (min.)	8' (min.)	NA	NA
Amenity Zone							
Primary Pedestrian Street	6' (min.)	6' (min.)	6' (min.)	6' (min.)	4' (min.)	NA	NA
All other Streets	4' (min.)	4' (min.)	4' (min.)	4' (min.)	4' (min.)	NA	NA
Sidewalk (clear width)^u						-	-
Primary Pedestrian Street	6' (min.)	6' (min.)	6' (min.)	6' (min.)	6' (min.)	NA	NA
All other Streets	4' (min.)	4' (min.)	4' (min.)	4' (min.)	4' (min.)	NA	NA

*Standard ranges for width provided. Max. ROW widths may vary based on the Streetscape Zone. ROW widths for dedication along existing Streets shall be based on the city's adopted Thoroughfare Plan and LDR.

** Total pavement widths shall be sufficient for fire access

#Unless alley acts as a fire lane, then fire lane standards shall apply

^e Less than 6,000 cars/day and non Primary Pedestrian Street frontage

*** Shall be based on any adopted bike plans

^u Any enhanced sidewalks shall meet the city's adopted Trail Plan standards

Appendix F Transition Sub-district Street Design Standards

Street Types →	Arterial	Collector (Divided)	Collector (Undivided)	Local Street		Alley	
				Commercial	Residential	Commercial	Residential
ROW* (min-max)	76'-106'	70'-112'	36'-87'	38'-80'	42'-58'	32'	18'
Design Speed	45 MPH	35-45 MPH	25-35 MPH	25 MPH	25 MPH	<20 MPH	<20 MPH
Travel Zone							
Number of Lanes (min-max)	4-6	4	2-3	2	2	2	1 Yield
Lane Widths** (min-max)	11'-12'	10'-11'	10'-11'	10'-11'	10'-11'	11'	Yield# (12'-15')
Median Required (Y/N; min-max)	Y (14'-16')	Y (12'-14')	N	N	N	N	N
On-Street Parking Permit'd (Y/N)	N	Y	Y	Y	Y	N	N
Allowable Parking design and Min. Width	Parallel (8' min.)	-	Y	Y	Y	-	-
	Angled (60 degree, 18' min.)	-	Y	Y	Y	N	-
	Head-in (20' min.)	-	N	N	Y ^ε	N	-
On-Street Bike Lane*** (Permitted Y/N)	-N	Y	Y	Y	N-	-N	-N
Streetscape Zone							
Total Streetscape Zone	9' (min.)	9' (min.)	9' (min.)	9' (min.)	9' (min.)	NA	NA
Amenity Zone	4' (min.)	4' (min.)	4' (min.)	4' (min.)	4' (min.)	NA	NA
Sidewalk (clear width)^μ	5' (min.)	5' (min.)	5' (min.)	5' (min.)	5' (min.)	- NA	- NA

*Standard ranges for width provided. Max. ROW widths may vary based on the Streetscape Zone.. ROW widths for dedication along existing streets shall be based on the city's adopted Thoroughfare Plan and LDR

** Total pavement widths shall be sufficient for fire access

#Unless alley acts as a fire lane, then fire lane standards shall apply

^ε Less than 6,000 cars/day and non Primary Pedestrian Street frontage

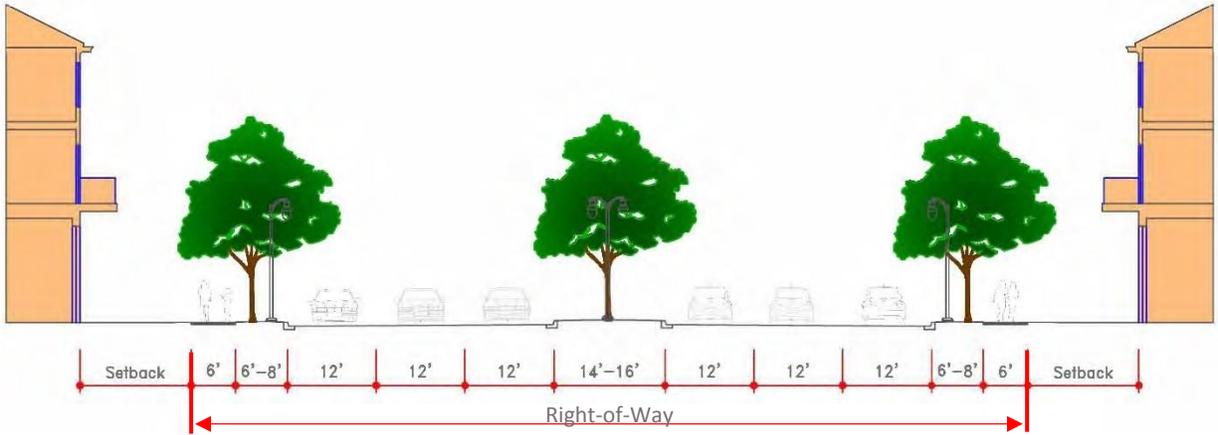
*** Shall be based on any adopted bike plans

^μ Any enhanced sidewalks shall meet the city's adopted Trail Plan standards

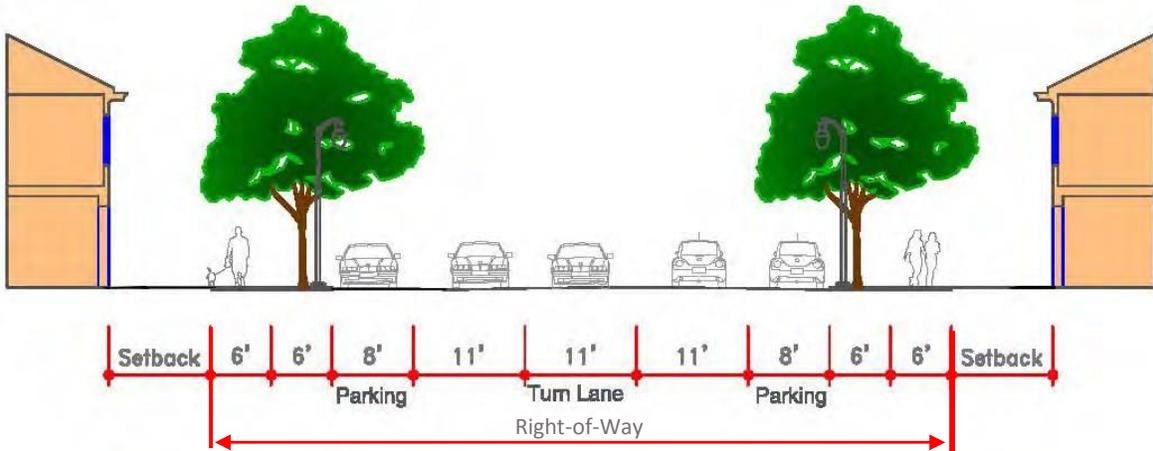
Example Core District Cross Sections

The following street cross sections are examples developed from the Street Design Standards for Street types in the Corridor District. These are provided to show how elements can be combined. ROW widths shall be derived from the Master Thoroughfare Plan, where applicable, and Street Design Standards.

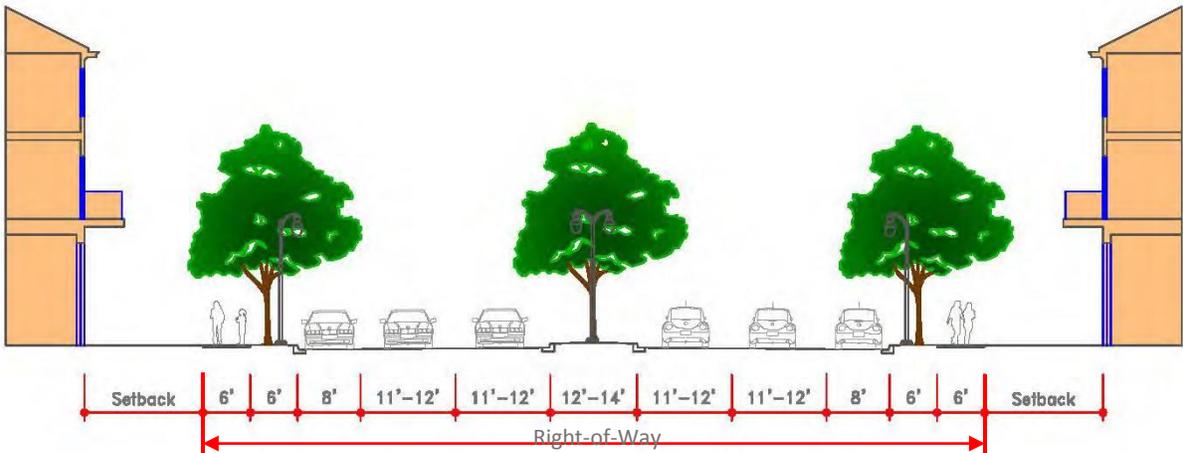
Arterial



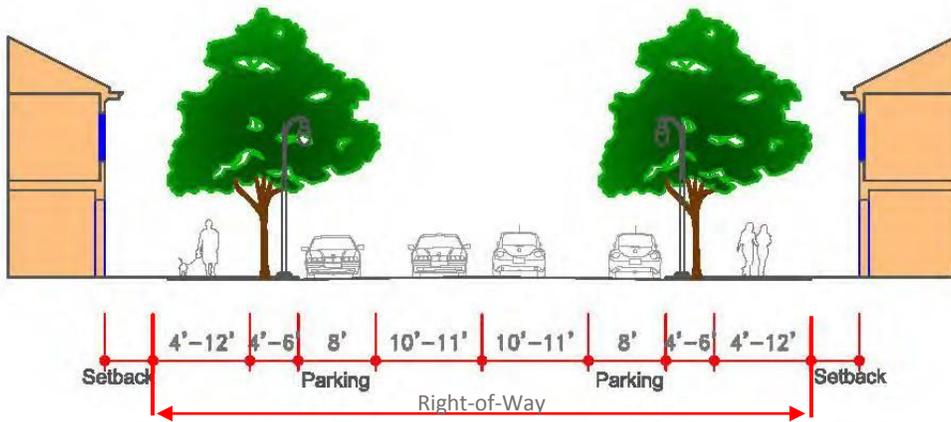
Collector Undivided



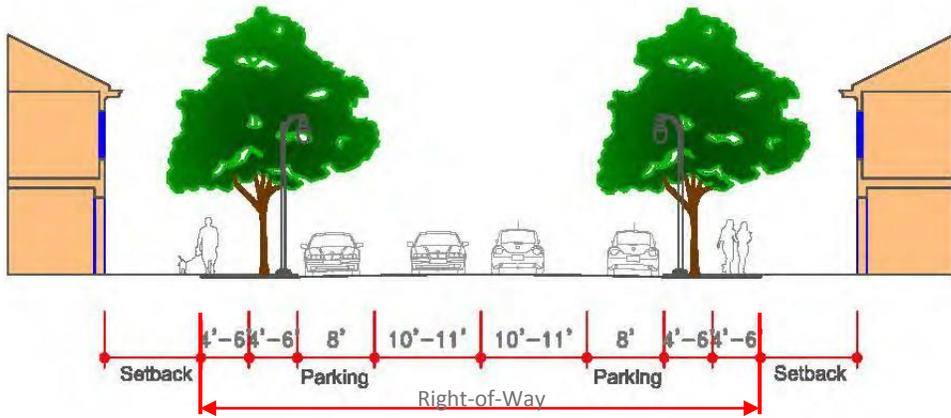
Collector Divided



Commercial Street (Local Street)



Residential Street (Local Street)



Commercial Alley	Residential Alley
<p>Dimensions (from left to right): - Setback: 5' - Lane: 11' - Lane: 11' - Setback: 5'</p>	<p>Dimensions (from left to right): - Setback: 3'-5' - Lane: 12'-15' - Setback: 3'-5'</p>

Appendix G Approved Plant Material List

The following lists contain all species approved for use in the Corridor District. It contains native and acceptable adapted species. Other species that are drought tolerant and adaptive may be used for planting within the Corridor District with approval of an Administrative Modification, as specifically authorized in this chapter.

CANOPY TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Live Oak	<i>Quercus virginiana</i>
Red Oak *	<i>Quercus shumardi</i>
Bald Cypress	<i>Taxodium distichum</i>
Sweetgum	<i>Liquidambar styraciflua</i>
Cedar Elm	<i>Ulmus crassifolia</i>
Lacebark Elm *	<i>Ulmus parvifolia</i>
Bigtooth Maple*	<i>Acer grandidentatum</i>
Caddo Maple	<i>Acer saccharum 'Caddo'</i>
Texas Ash*	<i>Fraxinus velutina 'Rio Grande'</i>
Bur Oak	<i>Quercus macrocarpa</i>
Chinquapin Oak	<i>Quercus muhlenbergii</i>
Escarpment Live Oak	<i>Quercus fusiformis</i>
Ginkgo	<i>Ginkgo biloba</i>
Pecan	<i>Carya illinoensis</i>

**Species preferred but not required for Amenity Zones*

ORNAMENTAL TREE LIST

<u>Common Name</u>	<u>Botanical Name</u>
Yaupon Holly	<i>Ilex vomatoria</i>
Crape Myrtle	<i>Lagerstromia indica</i>
Deciduous Yaupon	<i>Ilex decidua</i>
Southern Crabapple	<i>Malus app.</i>
Mexican Plum	<i>Prunus Mexicana</i>
Wax Myrtle	<i>Myrica carifera</i>
Chitalpa	<i>Chitalpa tashkentensis</i>
Deciduous Holly	<i>Ilex decidua</i>
Desert Willow	<i>Chilopsis linearis</i>
Eve's Necklace	<i>Sophora affinis</i>

SHRUBBERY LIST

<u>Common Name</u>	<u>Botanical Name</u>
Dwarf Nandina	<i>Nandina domestica 'nana'</i>
Dwarf Burford Holly	<i>Ilex cornuta 'burfordi nana'</i>
Abelia Grandiflora	<i>Abelia grandiflora</i>
Barberry	<i>Barberry spp.</i>
Yucca (Red, Yellow or Soft Tip)	<i>Hesperaloe parviflora</i>
Texas Sage	<i>Leucophyllum frutescans</i>
Indian Hawthorn	<i>Raphiolepis indica</i>
Dwarf Crape Myrtle	<i>Lagerstromia indica 'nana'</i>

Common Name

Dwarf Yaupon Holly
Pampas Grass
Black-Eyed Susan
Dwarf Wax Myrtle
Needlepoint Holly
Knockout Rose
Rosemary

Botanical Name

Ilex vomitorria 'nana'
Cortaderia selloana
Rudbeckia hirta
Myrica pusilla
Ilex cornuta 'Needle Point'
Rosa 'Knock Out'
Rosmarinus officinalis

GROUND COVER/VINES LIST

Common Name

Asian Jasmine
Big Blue Liriope
Mondograss
Purple Winter Creeper
Santolina
Trumpet Vine
Virginia Creeper
Lady Banks Rose
Confederate Jasmine
Crossvine
Evergreen Wisteria
Lantana 'New Gold'
Liriope 'Silver Dragon'
Prostrate Rosemary
Sweet Autumn Clematis

Botanical Name

Trachelosperum Asiaticum
Liriope muscari
Ophiopogon japonicus
Euonymum coloratus
Santolina virens
Campsis radicans
Parthenocissus quinquefolia
Rosa banksiaw lutea
Trachelospermum jasminoides
Bignonia capreolata
Millettia reticulata
Lantana camara 'New Gold'
Liriope muscari 'Silver Dragon'
Rosmarinus officinalis prostrata
Clematis terniflora

ORNAMENTAL GRASSES LIST

Common Name

Dwarf Fountain Grass 'Little Bunny'
Dwarf Maiden Grass
Fountain Grass
Inland Seaoats
Maiden Grass
Mexican Feather Grass
Muhly Grass
Weeping Lovegrass

Botanical Name

Pennisetum alopecuroides 'Little Bunny'
Miscanthus sinensis 'Adagio'
Pennisetum alopecuroides
Chasmanthium latifolium
Miscanthus sinensis 'Gracillimus'
Stipa tenuissima
Muhlenbergia capillaris
Eragrostis curvula

TURF

Common Name

Bermuda
St. Augustine
Zoysia

Botanical Name

Cynodon dactylon
Stenotaphrum secundatum
Zoysia tenuifolia

Appendix H - IH-35E Corridor Overlay District Boundary

The IH-35E Corridor Overlay District is defined as all land located within the area bounded by the following: Beginning at the southwest corner of the intersection of North Stemmons Freeway (IH-35E) and North Garden Ridge Boulevard, running westerly along the south right-of-way line of North Garden Ridge Boulevard to the northwest corner of a 3.221-acre tract of land out of the F. Pierce Survey, Abstract No. 1016 conveyed to IH-35 & Garden Ridge Acquisitions, LP, then southerly and easterly along the western and southern boundaries of said IH-35 & Garden Ridge Acquisitions, LP tract to the west right-of-way line of North Stemmons Freeway, (IH-35E), then southerly along the west right-of-way line of North Stemmons Freeway (IH-35E) and crossing Bogard Lane to the southeast corner of the intersection of Bogard Lane and McGee Lane, then southerly along the east right-of-way line of McGee Lane to the northeast corner of the intersection of McGee Lane and the KCS Railroad, then easterly along the north right-of-way line of the KCS Railroad to the northeast corner of the intersection of the KCS Railroad and North Summit Avenue, then southerly along the east right-of-way line of North Summit Avenue to the northwest corner of Lot 1, Block H, Valley Ridge Residential Addition, then easterly along the north boundaries of Lot 1, Block H, Valley Ridge Residential Addition and Lot 3, Block I, Valley Ridge Business Park West Phase 9 to the west right-of-way line of North Stemmons Freeway (IH-35E), then southerly along the east boundary and westerly along the south boundary of Lot 3, Block I, Valley Ridge Business Park West Phase 9, then westerly along the south boundary of Lot 1, Block H, Valley Ridge Residential Addition to the north right-of-way line of College Parkway, then crossing the right-of-way College Parkway in a southwesterly direction to the eastern corner of Lot 1, Block A, Parkway Estates Addition, then in a southwesterly direction along the eastern and southern boundaries of Lot 1, Block A, Parkway Estates Addition to the southeast corner of a drainage right-of-way dedicated on the final plat of Fireside Village Addition, then westward along the southern boundary of said drainage right-of-way to the eastern right-of-way line of North Summit Avenue, then southward along the east right-of-way line of North Summit Avenue crossing West Main Street and continuing southerly along the east right-of-way line of Surf Street to the southwest corner of Lot 3, Block B, Harbor Heights, then eastward along the south boundary of Lot 3, Block B, Harbor Heights to the southeast corner of said lot, then northward along the east boundary of Lots 3, 2, and 1A, Block B, Harbor Heights to the southwest corner of Lot 6 (PT), Valley Square, then easterly along the south boundary of Lots 6 (PT), 3 and 2, Valley Square to a point being the intersection of the south boundary of Lot 2, Valley Square and the northward extension of the western boundary of Lot 5, Block G, Quailcrest, then south across the public alley to the northwest corner of Lot 5, Block G, Quailcrest and continuing southerly along the west boundaries of Lots 5, 4B, 3, 2 and 1, Block G, Quailcrest to the north right-of-way line of West Purnell Street, then east along the north right-of-way line of West Purnell Street to the west right-of-way line of South Stemmons Freeway (IH-35E), then southeasterly along the west right-of-way line of South Stemmons Freeway (IH-35E) to the northwest corner of a 0.9759-acre tract of land out of the J. Sutton Survey, Abstract No. 1155 conveyed to Neville Texas Investments, LLC, then south along the west boundary of said Neville Texas Investments LLC tract and continuing south along the west boundary of a 4.0839-acre tract and an 8.141-acre tract, both out of the J. Sutton Survey, Abstract No. 1155 conveyed to AVB Ltd, to the north right-of-way line of Fox Avenue, then in a

southeasterly direction across the Fox Avenue right-of-way to the northwest corner of Lot 1, Block A, Fox Haven Addition, then south and easterly along the west boundary and south boundary of Lot 1, Block A, Fox Haven Addition to the west boundary of Lot 6R, Block A, Fox-IH35E Addition, then southeasterly, east, and northeasterly along the west, south and southeast boundaries of Lot 6R, Block A, Fox-IH35E Addition to the west right-of-way line of South Stemmons Freeway (IH-35E), then southeasterly along the west right-of-way line of South Stemmons Freeway (IH-35E) to the north-most corner of Lot 3 (PT), Block A, Lakeland Plaza Addition, then southwesterly and southerly along the northwest and west boundaries of Lots 3 (PT) and 1 (PT), Block A, Lakeland Plaza Addition to the northwest corner of Lot 1, Block A, Peppermint Addition, then south along the west boundary of Lot 1, Block A, Peppermint Addition to the north right-of-way line of Oakwood Lane, then easterly, southerly and easterly along the north, east and north right-of-way lines (following curves) of Oakwood Lane to the east right-of-way line of SH 121 Business, then south along the east right-of-way line of SH 121 Business to the southwest corner of Lot 1, Block A, Rider Addition, then east along the south boundaries of Lot 1, Block A, Rider Addition and Lot 1R, Block A, Bankston Nissan Addition to the west boundary of Lot 1, Block A, Lewisville Mitsubishi Addition, then south along the west boundaries of Lot 1, Block A, Lewisville Mitsubishi Addition and Lot 1, Block A, Toyota of Lewisville to the southwest corner of Lot 1, Block A, Toyota of Lewisville, then east along the south boundary of Lot 1, Block A, Toyota of Lewisville to the northwest corner of Lot 1A, Block C, Timber Creek Square Phase I, then south along the west boundary of Lot 1A, Block C, Timber Creek Square Phase I to the northeast right-of-way line of East Southwest Parkway, then southeasterly along the northeast right-of-way line of East Southwest Parkway to the south right-of-way line of East Corporate Drive, then southwesterly along the south right-of-way line of East Corporate Drive to the north most corner of Lot 2, Block C, Timber Creek Square Phase 2, then southeasterly along the northeast boundary of Lot 2, Block C, Timber Creek Square Phase 2 to the northwest corner of Lot 6, Block A, Vista Ridge Pointe Addition, then south along the west boundaries of Lot 6, Block A, Vista Ridge Pointe Addition and Lots 2 and 1A, Block B, Vista Ridge Village Addition Phase 3 to the north right-of-way line of Oakbend Drive, then in a southeasterly direction across the Oakbend Drive right-of-way to the northwest corner of Lot 1, Block A, Oakbend and Rockbrook Addition, then south and east along the west and south boundaries of Lot 1, Block A, Oakbend and Rockbrook Addition to the west boundary of Lot 4, Block A, Vista Ridge Village Addition Phase 2, then south along the west boundaries of Lots 4, 3 and 2, Block A, Vista Ridge Village Addition Phase 2 to the north right-of-way of East Round Grove (FM 3040), then east along the north right-of-way line of Round Grove Road (FM 3040) to the east right-of-way line of Mac Arthur Boulevard, then southerly along the east right-of-way line of MacArthur Boulevard to the north right-of-way line of SH 121, then easterly along the north right-of-way line of SH 121 to the east right-of-way line of Lake Vista Drive, then southerly and easterly along the east and north right-of-way line of Lake Vista Drive to the south most corner of Lot 1RA, Block L, Vista Ridge to the southwest corner of a 38.0408-acre tract of land out of the McKinney-Williams Survey, Abstract No. 936 conveyed to Dallas Gun Club, then east along the south boundary of said Dallas Gun Club tract and following the City of Lewisville corporate limit line along the south boundary of a 2.2-acre tract of land out of the McKinney-Williams Survey, Abstract No. 936 and conveyed to Denton County Levee Improvement District No. 1 and continuing east and following the City

of Lewisville corporate limit line along the south boundary of said Dallas Gun Club tract and continuing east following the City of Lewisville corporate limit line along the south boundary of a 5.8-acre tract of land out of the McKinney-Williams Survey, Abstract No. 936 conveyed to Denton County Levee Improvement District No. 1 and continuing east following the City of Lewisville corporate limit line along the south boundary of a 38.0408-acre tract of land out of the McKinney-Williams Survey, Abstract No. 936 conveyed to Dallas Gun Club to the west most southwest corner of Lot 6R, Block A, I-35 Boat City Addition, then east, south, easterly, northerly, west and north (following curves) along the south, west, south, east, north and east boundaries of Lot 6R, Block A, I-35 Boat City Addition to south corner of Lot 5, Block A, I-35 Boat City Addition, then northeasterly along the southeastern boundary of Lot 5, Block A, I-35 Boat City Addition to the west right-of-way line of South Stemmons Freeway (I-35E), then following the City of Lewisville corporate limit line northeasterly across the South Stemmons Freeway (IH-35E) right-of-way to the east right-of-way line of South Stemmons Freeway (IH-35E) then following the City of Lewisville corporate limit line northwesterly to the southeast corner of a 14.029-acre tract of land out of the McKinney-Williams Survey, Abstract No. 936 conveyed to City of Carrollton, then northwesterly and east following the City of Lewisville corporate limit line along the southwest, west and north boundaries of said City of Carrollton tract to the west boundary of 109.6-acre tract of land out of the J. Chowning Survey, Abstract No. 242 conveyed to City of Carrollton, then north following the City of Lewisville corporate limit line along the west boundary of said City of Carrollton tract to the west right-of-way line of the Dallas Area Rapid Transit (DART) railway, then northerly along the west right-of-way line of the Dallas Area Rapid Transit (DART) railway, crossing the SH 121 right-of-way, to the north right-of-way line of SH 121, then southwesterly along the north right-of-way line of SH 121 to the southeast corner of Lot 4 (PT), Block A, Arthur James Addition, then northwesterly along the northeastern boundary of Lots 4 (PT), 3 and 2, Block A, Arthur James Addition to the east right-of-way line of Arthur's Lane, then northwesterly across the Arthur's Lane right-of-way to northeast corner of Lot 8R, Block H, Lakepointe Phase 5, then northwesterly along the northeast boundary of Lots 8R, 7B (PT) and 7A (PT), Block H, Lakepointe Phase 5 to the southeast corner of Lot 3, Block H, Lakepointe Phase 1, then northwesterly and northeasterly along the southwest and northwest boundaries of Lot 3, Block H, Lakepointe Phase 1 to the northmost corner of Lot 3, Block H, Lakepointe Phase 1, then in a northeasterly direction across Lot 3, Block G, Lakepointe Phase 3 to the southeast corner of Lot 2R-1, Block G, Lakepointe Phase 3, then northeasterly along the southeast boundaries of Lots 2R-1 and 1R-1B, Block G, Lakepointe Phase 3 and continuing in a straight line across the right-of-way of Lakeside Circle to the north right-of-way line of Lakeside Circle, then northwesterly along the north right-of-way line of Lakeside Circle to the south corner of Lot 1R-2A, Block E, Lakepointe Phase 3, then northeasterly along the southeast boundaries of Lots 1R-2A and 1R-3, Block E, Lakepointe Phase 3 and Lots 1, 2R1, 3R1 and 4, Block A, Hebron Lakepointe Addition to the south right-of-way line of Hebron Parkway, then crossing the right-of-way of Hebron Parkway to the east corner of Lots 3B and 3C, Block D, Lakepointe Phase 3, then northwesterly and southwesterly along the northeast boundary of Lots 3B and 3C, Block D, Lakepointe, Phase 3 and the northeast and northwest boundaries of Lots 3A (E PT) and 3A (W PT), Block D, Lakepointe Phase 3 to the northeast right-of-way line of Lakepointe Drive, then crossing the right-of-way of Lakepointe Drive to the southwest right-of-way line of Lakepointe Drive, then

northwesterly along the southwest right-of-way line of Lakepointe Drive to the south corner of the intersection of Lakepointe Drive and Waters Ridge Drive, then southwesterly along the southeast right-of-way line of Waters Ridge Drive to the west corner of Lot 7, Block A, Lakepointe West, then northwesterly across the right-of-way Waters Ridge Drive to the south corner of Lot 4A, Block B, Lakepointe Phase 2, then northwesterly, west and north along the southwest, south, and west boundaries of Lot 4A, Block B, Lakepointe Phase 2 to the south right-of-way line of Lakepointe Drive, then northwesterly along the south and west right-of-way line of Lakepointe Drive and crossing East Corporate Drive and continuing northwesterly along the southwest right-of-way line of Lakepointe Drive to a point being the intersection of the south right-of-way line of Lakepointe Drive and the southerly extension of the west boundary of Lot 5, Block A, Xerox Facility Addition, then north across the right-of-way of Lakepointe Drive to the southwest corner of Lot 5, Block A, Xerox Facility Addition, then north along the west boundary of Lot 5, Block A, Xerox Facility Addition to the south right-of-way line of Bennett Lane, then northeasterly across the Bennett Lane right-of-way to the southeast corner of a 0.963-acre tract of land out of the E. Aday Survey, Abstract No. 11 conveyed to City of Lewisville, then north to the northeast corner of said City of Lewisville tract then west along the north boundaries of said City of Lewisville tract; and continuing west, north and west along the north boundaries of a 1.926-acre tract of land out of the E. Aday Survey, Abstract No. 11 conveyed to City of Lewisville; Lot 1, Block A, Bennett Park Center; Lot 1, Block A, Octavi Hernandez Addition; a 0.75-acre tract of land out of the E. Aday Survey, Abstract No. 11 conveyed to Noblitt Living Trust; Lot 1, Block A, Tyson Estates; a 0.959-acre tract of land out of the E. Aday Survey, Abstract No. 11 conveyed to E&H Contractors; a 3.0-acre tract of land out of the E. Aday Survey, Abstract No. 11 conveyed to Rafael Corona; and a 3.027-acre tract of out of the E. Aday Survey, Abstract No. 11 conveyed to Nancy A. Stevens; to the east right-of-way line of Yates Street, then northerly along the east right-of-way line of Yates Street to the northwest corner of Lot 1, Block A, WRN Creekside Development Addition, then east, north and west along the south, east and north boundaries of a 57.3366-acre tract of land out of the J.W. King Survey, Abstract No. 695 conveyed to AMC Northwood, LLC, to the southwest corner of Lot 2, Block A, DCTA Maintenance Facility Addition, then north and east along the west and north boundaries of Lot 2, Block A, DCTA Maintenance Facility Addition to the southeast corner of 3R, Block A, Zander Place Addition, then north and northwesterly along the east and northeast boundaries of Lot 3R, Block A, Zander Place Addition to the southeast right-of-way line of SH 121 Business, then continuing in a straight line northwest across the SH 121 Business right-of-way to the northwest right-of-way line of SH 121 Business, then southwesterly along the northwest right-of-way line of SH 121 Business to the west corner of the intersection of SH 121 Business and McKenzie Street right-of-ways, then west along the south right-of-way line of McKenzie Street to the northeast corner of Lot 7, Block 10, McKenzie-Hembry Addition, then southerly, westerly and southerly along the east boundary of Lot 7, Block 10, McKenzie-Hembry Addition to the northeast corner of a 1.45-acre tract of land out of the J.W. King Survey, Abstract No. 695 conveyed to City of Lewisville, then southwesterly along the southeast boundaries of said City of Lewisville tract to the north-most corner of a 2.44-acre tract of land out of the J.W. King Survey, Abstract No. 695 conveyed to City of Lewisville, then southeasterly, southwesterly, westerly and northwesterly along the northeast, southeast, south and southwest boundaries of said City of Lewisville tract to the east right-of-way line of South Mill Street, then north along the east

right-of-way line of South Mill Street to the northeast corner of South Mill Street and Mesquite Street, then westerly across the South Mill Street right-of-way to the northeast corner of a 0.482-acre tract of land out of the E. Pickett Survey, Abstract No. 1014 conveyed to Lebz Brothers Inv. LLC, then west and south along the north and west boundaries of said Lebz Brothers Inv. LLC tract to the north right-of-way line of Yale Avenue, then southerly across the Yale Avenue right-of-way to the northwest corner of Lot 17, Block A, Termin, then south along the west boundaries of Lot 17, Block A, Termin and continuing southerly along the west boundary of a 0.45-acre tract of land out of the E. Pickett Survey, Abstract No. 1014 conveyed to Reid M. Anderson to the north right-of-way line of Harvard Avenue, then south across the Harvard Avenue right-of-way to the northwest corner of a 0.441 acre tract of land out of the E. Pickett Survey, Abstract No. 1014 conveyed to Bruce Properties LLC, then south along the west boundary of said Bruce Properties LLC tract to the north boundary of Lot 1, Block A, Batterton Addition, then west along the north boundaries of Lot 1, Block A, Batterton Addition and continuing west along the north boundaries of Lot 2, Block B, Huffines Dodge Addition, and a 3.7969-acre tract of land out of the E. Pickett Survey, Abstract No. 1014 conveyed to 1090 St. Charles Property, LLC, to the east right-of-way line of South Charles Street, then west across the South Charles Street right-of-way to the west right-of-way line of South Charles Street, then south along the west right-of-way line of South Charles Street to the north corner of a 0.711-acre tract of land out of the E. Pickett Survey, Abstract No. 1014 conveyed to Michael & Hiyasmin Noyes, then southwesterly along the northwest boundary of said Noyes tract to the east corner of Lot 9, Block B, Lakeland Terrace 1, then northwesterly along the northeast boundary of Lot 9, Block B, Lakeland Terrace 1 to the north corner of Lot 9, Block B, Lakeland Terrace 1, then northwesterly across the Lake Shore Drive right-of-way to the west right-of-way line of Lake Shore Drive, then north along the west right-of-way line of Lake Shore Drive to the southwest corner of Lake Shore Drive and Harbor Drive, then westerly, northwesterly and northerly along the south, southwest and west right-of-way line of Harbor Drive to the southwest corner of Harbor Drive and Lake Haven Drive, then north across the Lake Haven Drive right-of-way to the north right-of-way line of Lake Haven Drive, then northwesterly along the northeast right-of-way line of Lake Haven Drive to the east corner of Lake Haven Drive and Fox Avenue, then northeasterly across the right-of-way of Fox Avenue to the southeast corner of Lot 9, Block 4, James Degan, then westerly along the south boundaries of Lots 9, 8, 7, 6, 5, 4, 3 and 2 (PT), Block 4 James Degan to the northeast right-of-way line of South Stemmons Freeway (IH-35E), then northwesterly along the northeast right-of-way line of South Stemmons Freeway (IH-35E) to the northeast corner of South Stemmons Freeway (IH-35E) and West Purnell Street, then east along the north right-of-way line of West Purnell Street to the northwest corner of West Purnell Street and South Edna Avenue, then north along the west right-of-way line of South Edna Avenue to the southwest corner of South Edna Avenue and West Main Street, then north across the West Main Street right-of-way to the north right-of-way line of West Main Street, then west along the north right-of-way line of West Main Street to the northwest corner of West Main Street and Degan Avenue, then north along the west right-of-way line of Degan Avenue to the northwest corner of Degan Avenue and West College Street, then west along the north right-of-way line of West College Street to the northwest corner of Harn Drive, then north along the west right-of-way line of Harn Drive to the southwest corner of Harn Drive and Millican Drive, then north across the Millican Drive right-of-way to the north right-of-way line of

Millican Drive, then east along the north right-of-way line of Millican Drive to the southwest corner of Lot 18, Block E, Lewisville Park Estates, then north along the west boundaries of Lots 18, 17, 16, 15, 14, 13, 12, 11, 10, 9, 8, 7, 6, 5, 4, 3, 2 and 1, Block E and Lots 1-5, Block A Lewisville Park Estates to the northwest corner of Lot 5, Block A, Lewisville Park Estates, then east along the north boundaries of Lots 5-12, Block A, Lewisville Park Estates to the northeast corner of Lot 12, Block A, Lewisville Park Estates, then northeasterly across two drainage right-of-way lots to the southeast corner of Lot 3A, Block A, Valley Ridge Business Park East, then north along the east boundary of Lot 3A, Block A, Valley Ridge Business Park East to the northeast corner of Lot 3A, Block A, Valley Ridge Business Park East, then northwesterly across the Valley Ridge Boulevard right-of-way to the southeast corner of Lot 3, Block B, Valley Ridge Business Park East, then north along the east boundary of Lot 3, Block B, Valley Ridge Business Park East to the northeast corner of Lot 3, Block B, Valley Ridge Business Park East, then westerly along the south right-of-way line of the KCS railway to east corner of Lot 1, Block A, May Trailer Sales Addition, then northwesterly across the KCS Railway right-of-way to the southeast corner of Lot 3, Block A, Butler Addition, then north along the east boundaries of Lot 3, Block A, Butler Addition and Lot 1, Block A, Denton County Addition to the south right-of-way line of West Jones Street, then northerly across the West Jones Street right-of-way to the southeast corner of Lot 2R1, Block A, Fairway Business Park, then north along the east boundary and west along the north boundary of Lot 2R1, Block A, Fairway Business Park to the east boundary of Lot 1, Block A, Phil Dill Jr Addition, then north along the east boundaries of Lot 1, Block A, Phil Dill Jr Addition and Lot 2, Block A Lewisville Boat Storage Addition (PT) to the southwest right-of-way line of the Dallas Area Rapid Transit (DART) railway, then northwesterly along the southwest Dallas Area Rapid Transit (DART) railway right-of-way line to the northwest corner of Lot 2, Block A, Franklin Street Addition, then northeasterly across the Dallas Area Rapid Transit (DART) railway right-of-way to the southeast corner of Lot 1, Block A, CB Green Addition, then north along the east boundary of Lot 1, Block A, CB Green Addition to the south right-of-way line of Lake Park Road, then north across the Lake Park Road right-of-way to the north right-of-way line of Lake Park Road, then west along the north right-of-way line of Lake Park Road to the southeast corner of a 3.2258-acre tract of land out of the P.K. Waggoner Survey, Abstract No. 1342 conveyed to the James C. Holmes Family Trust, then north along the east boundary of said James C. Holmes Family Trust tract to southeast corner of Lot 2, Block A, Callmark Addition, then north and westerly along the east and north boundaries of Lot 2, Block A, Callmark Addition and continuing westerly along the north boundary of Lot 1, Block A, Callmark Addition to the east right-of-way line of Oakridge Boulevard, then northwesterly across the Oakridge Boulevard right-of-way to the west right-of-way line of Oakridge Boulevard, then south along the west right-of-way line of Oakridge Boulevard to the northeast corner of Lot 30A, Block A, Oakridge Park Estates Section G, then northwest, west and south along the northeast, north and west boundaries of Lot 30A, Block A, Oakridge Park Estates Section G to the northeast right-of-way line of the Dallas Area Rapid Transit (DART) railway, then crossing the Dallas Area Rapid Transit (DART) railway right-of-way in a southwesterly direction and continuing northwesterly along the southwest right-of-way line of the Dallas Area Rapid Transit (DART) railway to the southwest corner of the Dallas Area Rapid Transit (DART) railway right-of-way and North Garden Ridge Boulevard, then westerly across the North Stemmons Freeway (IH-35E) right-of-way to the point of beginning.